

Mayor
Tom French

17425 Ballinger Way NE
Lake Forest Park, WA 98155-5556
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Email: cityhall@cityofLFP.gov
www.cityofLFP.gov



Councilmembers
Lorri Bodi
Tracy Furutani
Larry Goldman
Paula Goode
Ellyn Saunders

July 11, 2024

Sound Transit Board
Sound Transit
401 S. Jackson Street
Seattle, WA 98104

RE: Stride S3 Line Project

Dear Mr. Gorman, Ms. Mestas, Mr. Constantine, Mr. Somers, Mr. Upthegrove, and Ms. Balducci:

As elected representatives for the City of Lake Forest Park (LFP), we wish to thank Mr. Gorman for his March 29, 2024, response to our letter last fall describing our city's technical, environmental, and design concerns with Sound Transit's (ST's) current plans for Bus Rapid Transit on Bothell Way through our city. Our letter was a follow up to our previous requests for design and engagement improvements extending back two years. This includes public meetings in 2023 with CEO Timm attended by many hundreds of Lake Forest Park residents, where she committed to examine alternatives to a full BAT lane through LFP and improve communications with our city and community.

At the outset, and for the record, we are compelled to clarify some of the background and context described in the letter.

- **Voter Approval:** While most of our citizens voted for ST3, and our city strongly supports improved public transit, the current plans differ in many significant ways from the proposal voted on. Notably, the plans submitted to the voters did not include a northbound/eastbound BAT lane through LFP. (There are, however, existing BAT lanes northbound from 165th Street, and of course, the full length southbound in LFP, which were included and which we continue to support.)
- **Timely Comments:** Our city's formal concerns have not come at the last minute but extend back to comments made by the city at the draft 60% design stage, the first time specific details were shared with the public. At that time, we requested (in writing) a pause in project design to examine alternatives that would provide transit benefits with less adverse impacts, as well as improved community engagement.
- **Disproportionate Impacts:** Regardless of the SEPA checklist prepared in 2022 at 30% design, our city has repeatedly documented the disproportionate impacts in LFP. While 13% of the BRT project is in LFP's 1.2 miles, 55% of the costs and most of the adverse impacts are here.

- Transit Time Benefits: We note that the travel time improvements ST estimates of 2.3 minutes from new BAT lane components through LFP only occur from 4-6 pm weekdays northbound. Recent review of the same data and methodology shows that the estimate includes improvements outside of the LFP corridor. When that segment is removed, adjusted benefits are only 1.5 minutes at those peak hours. And even the 1.5 minutes may be overstated since it fails to include traffic from the 46 driveways (private homes, condos, apartments, and businesses) necessarily turning into and out of the new BAT lane, an adjustment allowed by ST's methodology, but not used.

We sincerely appreciate the recent design details described in your letter, addressing LFP's retaining wall and tree ordinances, as well as the environmentally sensitive critical areas surrounding Bsche'tla Creek, a designated fish bearing stream that has already experienced major landslides. We also concur in shortening the bus stop at 165th Street to one bus length (incidentally, a bus stop not included in the voter-approved proposal).

Even with these modifications you describe, the project as designed remains hugely impactful on our city, already bounded by impacts from two major state highways. No other city will face such inequitable impacts. As a reminder, under the current plans:

- Our green residential corridor and city gateway will still become a concrete corridor with a nearly mile-long retaining wall that is as high as 16 feet in places.
- 110 mostly residential properties will be taken in whole or part, with 46 driveways that will necessarily turn into the proposed BAT lane, posing safety issues.
- Five acres of mature trees and shrubs (including 80 landmark trees) will still be removed, and 95,000 tons of dirt and debris will still be removed and trucked, creating heat island and greenhouse gas effects that erode city and regional climate action plans.
- Increased stormwater runoff, discharging into three fish streams, will occur from tree removal and the creation of 1.3 acres of new impervious surfaces at a time when federal and state mandates are imposing costly new restrictions on the city, especially from tires and road surfaces.

Further, changes to the intersection at 145th Street will make public transit along Bothell Way worse for LFP riders. Sound Transit busses will no longer serve this intersection at all, and the design will prevent King County Metro from serving it north/westbound. This will remove 75% of bus access from LFP's Southern Gateway neighborhood, which includes a large middle-housing development. Such a severe reduction will ripple into the future since current zoning calls for multiplexes and affordable housing in the area.

While ST staff are approaching 100% design, LFP's fundamental concerns remain unaddressed. Our continued request over the years -- which CEO Timm agreed to address in public meetings with over 450 people in attendance -- is a meaningful engagement examining design alternatives for transit improvements less impactful to the LFP environment and the community. The simplistic slide presentation using cherry-picked Google data shared recently by ST staff truly does not honor Ms. Timm's commitment in a meaningful way.

Under the circumstances, after a detailed analysis of ST data and documents and in consultation with our community, we are now formally proposing the following alternative design concept for BAT transit improvement through Lake Forest Park (see attached).

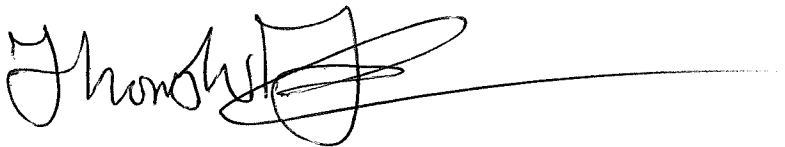
Starting at the south end, from Bothell Way at 145th to Ballinger Way, our proposal would be:

- Retain the new full BAT lane between 145th Street and 153rd Street, as now proposed by ST.
- Eliminate the proposed BAT lane between 153rd and 165th Streets, from traffic light to traffic light.
- Retain the existing BAT lane at 165th Street, as now proposed by ST
- Add signalized lights and lane jumps at 153rd and 165th Streets, allowing busses to merge into regular lanes north of the 153rd traffic light and merge into the existing BAT lane north of the 165th traffic light

Based on recent analyses, we think this is a preferred alternative for Bothell Way in many respects. It provides virtually identical transit time benefits to those from the full BAT lane currently proposed and relies on elements of the current ST designs for both Bothell Way and 145th. It would reduce or eliminate significant property takings and adverse environmental, greenhouse gas, and stormwater impacts, including construction in the vicinity of the Bsche'tla Creek critical/landslide area. As a result, it likely could be constructed faster, on time and on budget compared with current plans. We see this alternative as a win-win for Lake Forest Park and Sound Transit.

Looking ahead, we appreciate the renewed communication with ST leadership and look forward to continued collaboration. We will be following up seeking meetings with each of you in the month of July. We invite you to come and see the project area with us and meet our community at your convenience.

Sincerely,



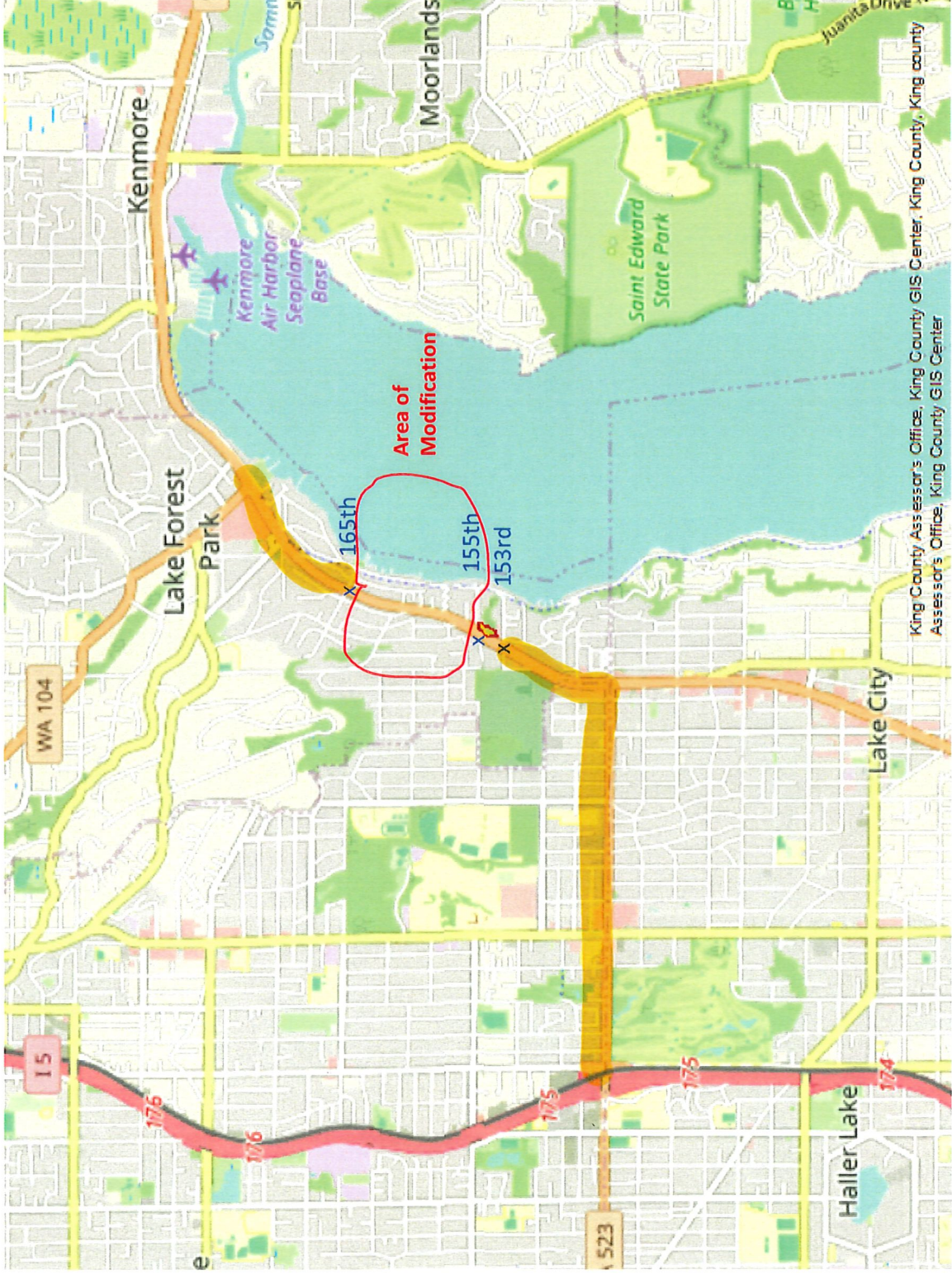
Tom French
Mayor



Lorri Bodi
Deputy Mayor on behalf of the following Councilmembers:

Tracy Furutani
Larry Goldman
Paula Goode
Ellyn Saunders

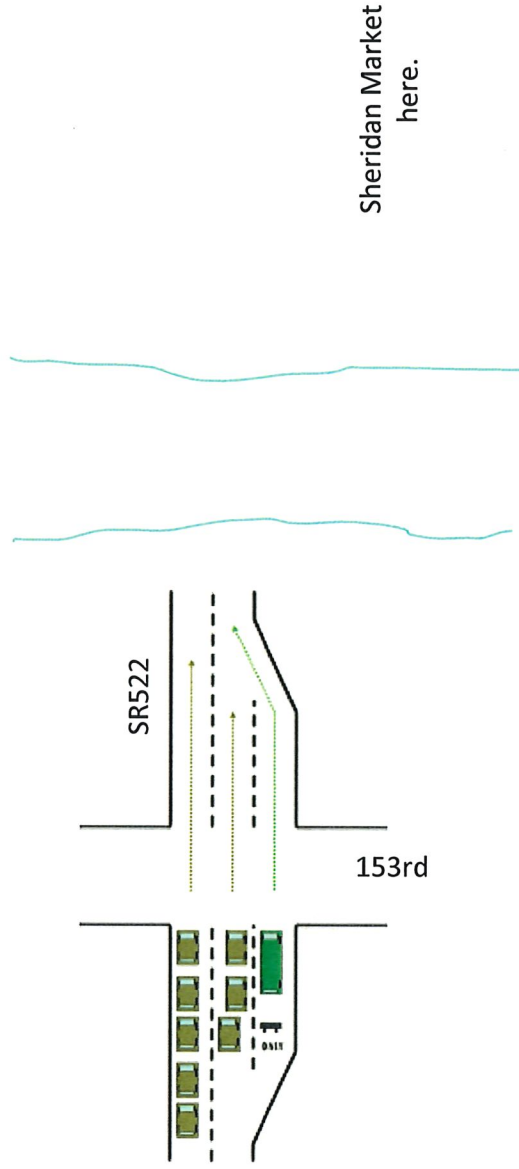
Map of Lake Forest Park



King County Assessor's Office, King County GIS Center, King County Assessor's Office, King County GIS Center

- Yellow line = SR522 Route
- Red = Bsche'tla Creek and Bridge
- X- Blue equals boundary of requested modification along route in LFP with NO WORK requested between 153rd and 155th due to liquification soil issues.

Jump Queue

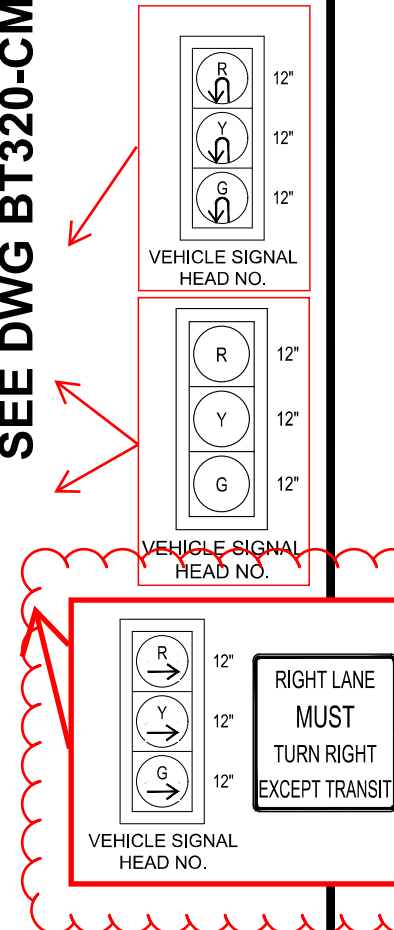
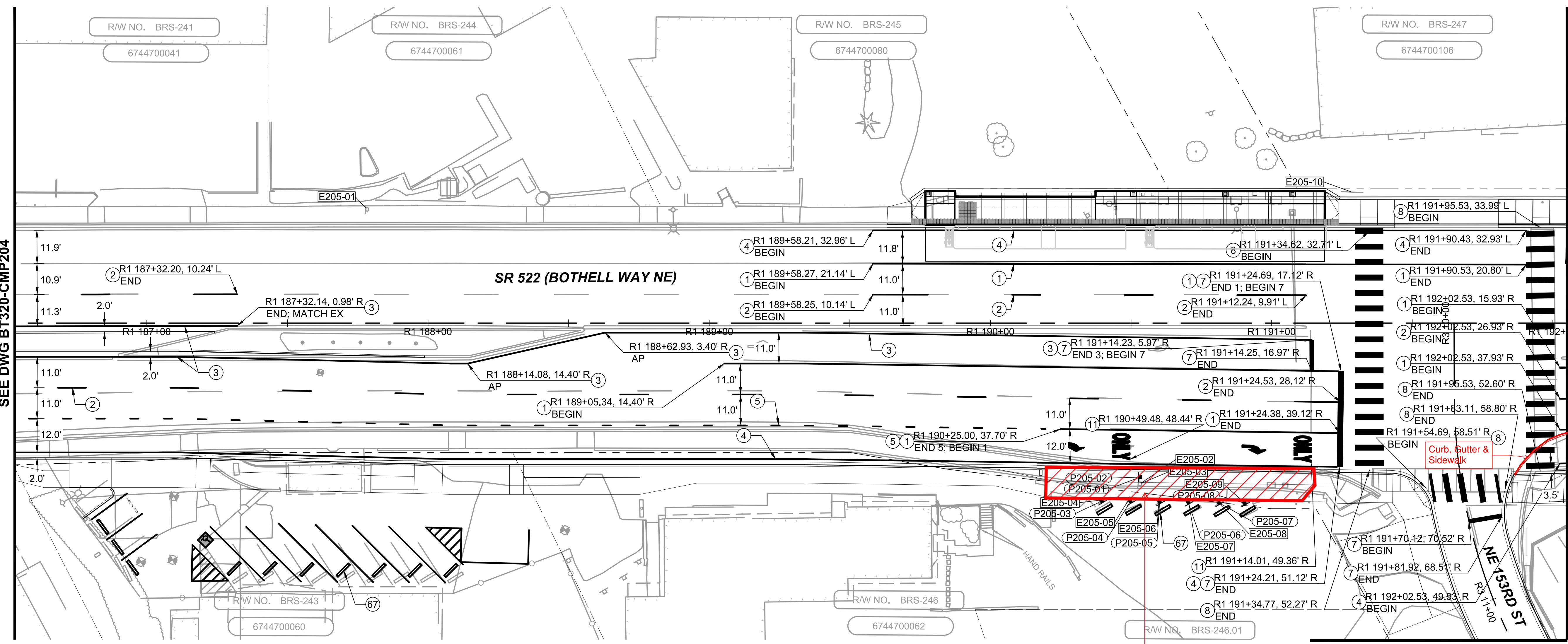


1. The image shows a 'Jump Queue' with a signalized traffic light. to be installed at 153rd.
2. This allows the bus to proceed ahead of traffic for the 10 blocks of the requested modification area. This is the SAME method that is being utilized on the ST3 project from I-5 to SR522.
3. At 165th, the route will resume as originally designed.

Xrefs:
 xS3-BT522-CRP301
 xS3-BT522-VRX101
 xBRT-CUD06-TB2234
 xS3-BT522-VSF100
 xS3-BT522-GZK101
 GB-SEAL-RW044721
 xS3-BT522-CMP301
 xS3-BT522-CMN301
 xS3-BT522-CAP301
 xS3-BT523-CAP201
 xS3-BT523-CMP201
 xS3-BT523-CRP201
 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZM001

MATCHLINE: R1 STA 186+53
 SEE DWG BT320-CMP204

MATCHLINE: R1 STA 192+05
 SEE DWG BT320-CMP206

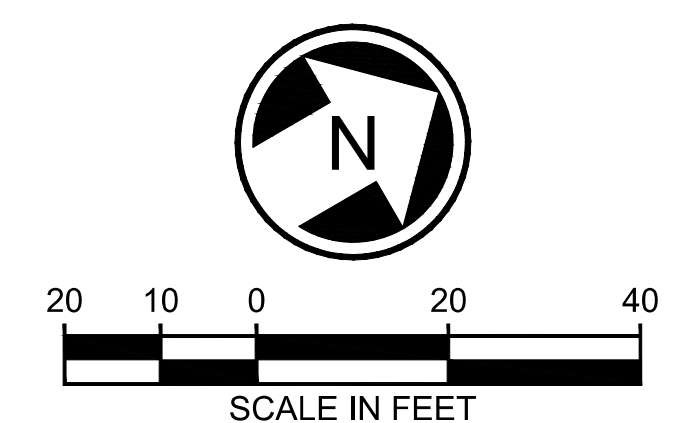


NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
 - ⑦ STOP LINE PER WSDOT STD. PLAN M-24.60
 - ⑧ CROSSWALK LINE PER WSDOT STD. PLAN M-15.10
 - ⑪ TYPE 2SR (RIGHT) TRAFFIC ARROW AND "ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10
 - ⑥7 CEMENT CONCRETE WHEEL STOP PER DETAIL ON SHEET BT330-CPD301

Suggested Modifications

1. Relocate bus stop prior to intersection to improve traffic flow with removed BAT lane.
2. Revise right turn lane signal to right arrows to allow for transit queue jump.
3. Revise roadway, curb, and sidewalk geometry to remove BAT lane north of NE 153rd St intersection



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90% SUBMITTAL
 NOT FOR CONSTRUCTION

DESIGNED BY:
O. NGUYEN
 DRAWN BY:
O. NGUYEN
 CHECKED BY:
M. KHURSHEED
 APPROVED BY:
L. OLSEN



Jacobs

STRIDE
 BRT General Engineering Consultant

LINE IS 1" AT FULL SCALE



SCALE:
1"=20'
 FILENAME:
S3-BT320-CMP201-210
 CONTRACT No.:
RTA/LR OA 0206-19
 DATE:
06/09/2023

SUBMITTED BY:
P. AVADUTHA

DATE:
06/09/2023

REVIEWED BY:
K. LEOTTA

DATE:
06/09/2023

SR 522 / NE 145TH BRT
SR 522 (BOTHELL WAY NE)
 BT306
 CIVIL
 PAVEMENT MARKING AND SIGNAGE PLAN
 R1 STA 186+53 TO R1 STA 192+05

DRAWING No.:
BT320-CMP205
 FACILITY ID:
BT320
 SHEET No.:
0733
 REV:

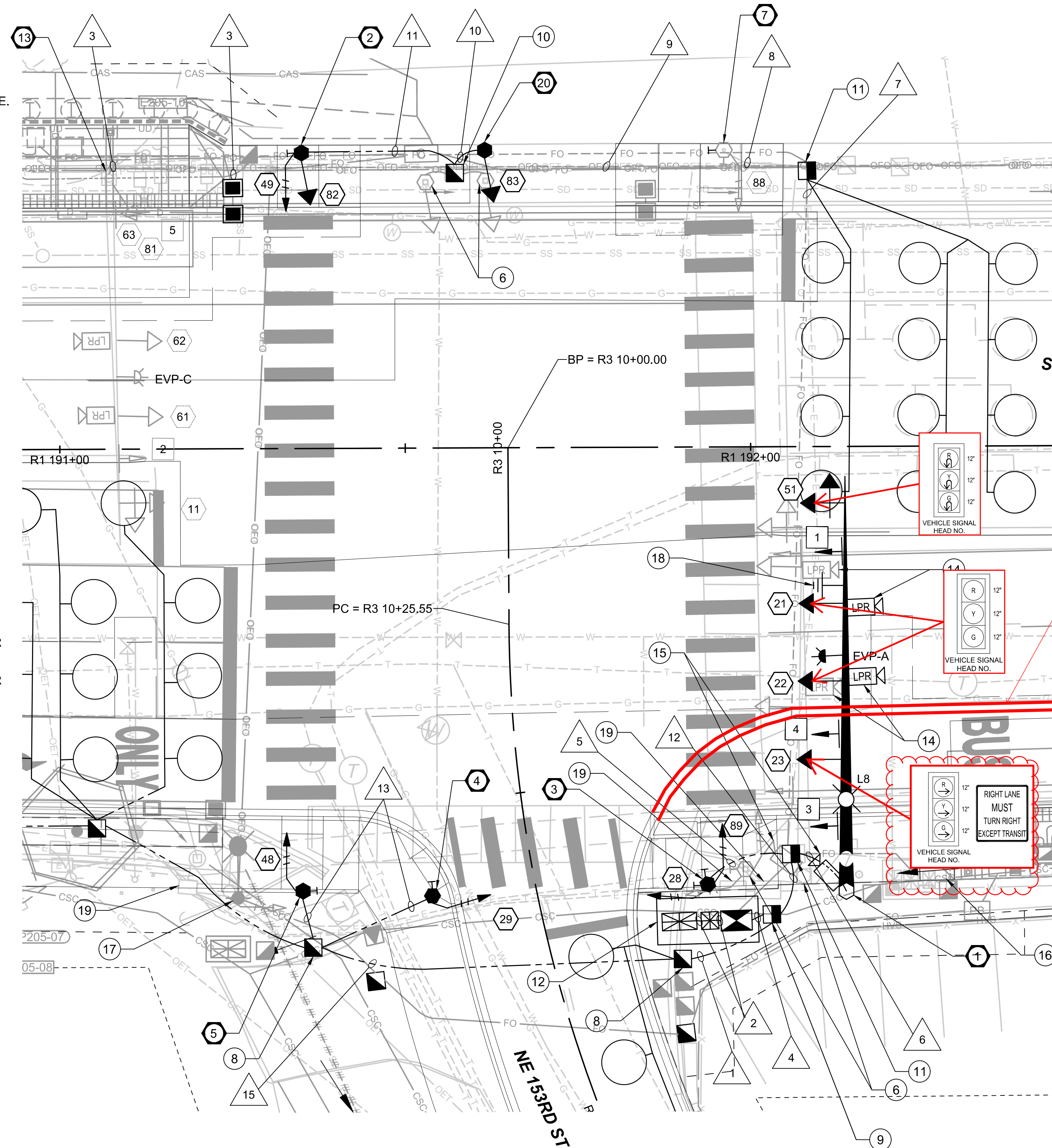
Xrefs:
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 xS3-BT522-LCP301
 xS3-BT522-TSP301
 xS3-BT522-RPP301
 xS3-BT522-CRP301
 xS3-BT522-CAP301
 xS3-BT522-CLP301
 xS3-BT522-CMP301
 xS3-BT522-CMP302
 xS3-BT522-CTP301
 xS3-BT522-APP301
 xS3-BT522-EPP301
 xS3-BT522-JIP301
 xS3-BT522-LPP301
 xS3-BT522-SWP301
 xS3-BT522-TIP301
 LFP Quantity Break-up

GENERAL NOTES:

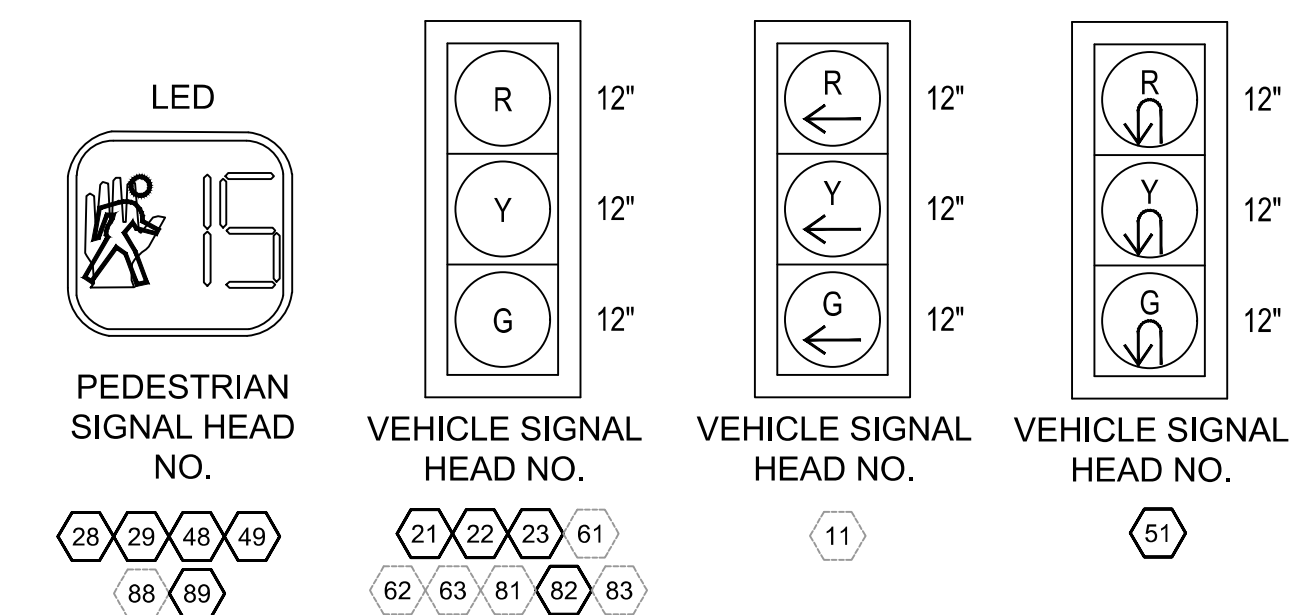
1. SEE SHEET BT320-TSN201 FOR LEGEND.
2. SEE SHEET BT320-CLP204 AND BT320-CLP205 FOR PROPOSED ILLUMINATION EQUIPMENT AND POWER SERVICE.
3. SEE SHEET BT320-TIP205 FOR PROPOSED COMMUNICATION EQUIPMENT.
4. SEE SHEET BT320-TSS201 FOR PROPOSED WIRING SCHEDULE.
5. SEE SHEET BT320-TSS202 FOR PROPOSED POLE SCHEDULE.
6. SEE SHEET BT320-TSD202 FOR ASSOCIATED DETECTION PLAN.
7. SEE SHEET BT320-TSS204 & BT320-TSS205 FOR PROPOSED TERMINATION SCHEDULE.

CONSTRUCTION NOTES:

1. CONSTRUCT FOUNDATION PER WSDOT STD PLAN J-26.10-03. FURNISH AND INSTALL TYPE III SIGNAL POLE. INSTALL ONE TERMINAL CABINET ON POLE. INSTALL FOUR VEHICLE HEADS, ONE EMERGENCY PRE-EMPTION, ONE DUEL CHANNEL TSP ANTENNA, ONE SPAT ANTENNA, TWO SIGNS, AND TWO RELOCATED LICENSE PLATE READERS ON MAST ARM. INSTALL ONE NEW LUMINAIRE FIXTURE ON LUMINAIRE ARM. SEE SHEET 522-TLP201 FOR WIRING DETAILS.
2. CONSTRUCT TYPE 1 POLE FOUNDATION PER WSDOT STD PLAN J-21.10-04. FURNISH AND INSTALL TYPE 1 POLE WITH ONE VEHICLE HEAD PER WSDOT STD PLAN J-21.15-01, ONE PEDESTRIAN HEAD PER WSDOT STD PLAN J-20.20-02 AND ONE APS PUSHBUTTON PER WSDOT STD PLAN J-20.26-01.
3. CONSTRUCT TYPE PS POLE FOUNDATION PER WSDOT STD PLAN J-21.10-04. FURNISH AND INSTALL TYPE PS POLE WITH TWO PEDESTRIAN HEADS PER WSDOT STD PLAN J-20.16-02 AND TWO APS PUSHBUTTONS PER WSDOT STD PLAN J-20.26-01.
4. CONSTRUCT TYPE PS POLE FOUNDATION PER WSDOT STD PLAN J-21.10-04. FURNISH AND INSTALL TYPE PS POLE WITH ONE PEDESTRIAN HEAD PER WSDOT STD PLAN J-20.16-02 AND ONE APS PUSHBUTTON PER WSDOT STD PLAN J-20.26-01.
5. CONSTRUCT TYPE PS POLE FOUNDATION AS CURB INTEGRATED BASE PER WSDOT STD PLAN J-20.11-03. FURNISH AND INSTALL TYPE PS POLE WITH ONE PEDESTRIAN HEAD PER WSDOT STD PLAN J-20.16-02 AND ONE APS PUSHBUTTON PER WSDOT STD PLAN J-20.26-01.
6. REMOVE EXISTING POLE AND FOUNDATION. REMOVE AND SALVAGE EXISTING EQUIPMENT. REMOVE ALL ASSOCIATED CONDUITS AND WIRING.
7. REMOVE EXISTING PUSHBUTTON FROM EXISTING POLE. CONTRACTOR SHALL SEAL THE UNUSED HOLES. INSTALL NEW APS PUSHBUTTON ASSEMBLY ON EXISTING POLE. PROTECT IN PLACE EXISTING POLE AND PEDESTRIAN HEAD. REMOVE EXISTING WIRING AND REWIRE ALL EXISTING FEATURES.
8. FURNISH AND INSTALL TYPE 2 JUNCTION BOX PER WSDOT STD PLAN J-40.10-04.
9. FURNISH AND INSTALL TYPE 8 JUNCTION BOX PER WSDOT STD PLAN J-40.30-04.
10. REMOVE EXISTING JUNCTION BOX. FURNISH AND INSTALL TYPE 2 JUNCTION BOX PER WSDOT STD PLAN J-40.10-04. PROTECT IN PLACE ALL EXISTING CONDUITS.
11. REMOVE EXISTING JUNCTION BOX. FURNISH AND INSTALL TYPE 8 JUNCTION BOX PER WSDOT STD PLAN J-40.30-04. PROTECT IN PLACE ALL EXISTING CONDUITS CROSSING SR 522.
12. CONSTRUCT JOINT THREE FOUNDATION PER WSDOT STD PLAN J-10.10-04. FURNISH AND INSTALL NEW FULLY EQUIPPED TYPE 332D CONTROLLER CABINET WITH CONTROLLER, FIBER TERMINATION PANEL, SWITCH, AND ALL ANCILLARY EQUIPMENT TO ACHIEVE A FULLY FUNCTIONAL SIGNAL WITH OPERATION AS SHOWN ON PLAN, ONE NEW TYPE D SERVICE CABINET, ONE NEW UNINTERRUPTIBLE POWER SUPPLY CABINET, ALL ASSOCIATED EQUIPMENT AND CONNECT ALL FIELD WIRING. ORIENT ALL CABINET DOORS SO THAT THEY FACE SOUTH. VERIFY FINAL LOCATION WITH ENGINEER PRIOR TO POURING FOUNDATION.
13. PROTECT IN PLACE EXISTING POLE AND SIGNAL EQUIPMENTS. REMOVE EXISTING WIRING AND REWIRE ALL EXISTING FEATURES.
14. RELOCATE EXISTING LICENSE PLATE READER CAMERA TO NEW MAST ARM.
15. RELOCATE EXISTING CCTV CAMERA TO NEW POLE.
16. REMOVE EXISTING SIGNAL CONTROLLER, UNINTERRUPTIBLE POWER SUPPLY, SERVICE CABINET AND EXISTING JOINT CONCRETE PAD. ALL REMOVED EQUIPMENT SHALL RETURN TO WSDOT. CONTRACTOR SHALL PROVIDE TEMPORARY SIGNAL CONTROL DURING CONSTRUCTION AND KEEP SIGNALS FULLY OPERATIONAL THROUGHOUT CONSTRUCTION UNTIL CHANGE OVER TO NEW SIGNAL SYSTEM.
17. REMOVE AND SALVAGE EXISTING PEDESTRIAN SIGNAL HEAD AND PUSHBUTTON. REMOVE ALL ASSOCIATED CONDUITS AND WIRING.
18. INSTALL ONE STRIDE TSP GPS ANTENNA ON MAST ARM.
19. REMOVE EXISTING JUNCTION BOX.
20. CONSTRUCT TYPE 1 POLE FOUNDATION PER WSDOT STD PLAN J-21.10-04. FURNISH AND INSTALL TYPE 1 POLE WITH VEHICLE HEAD PER WSDOT STD PLAN J-21.15-01.
21. INSTALL TYPE 3 INDUCTION LOOP VEHICLE DETECTORS. WIRING SHALL BE IN ACCORDANCE WITH WSDOT STD PLAN J-50.12-02.
22. FURNISH AND INSTALL TYPE 1 JUNCTION BOX PER WSDOT STD PLAN J-40.10-04.



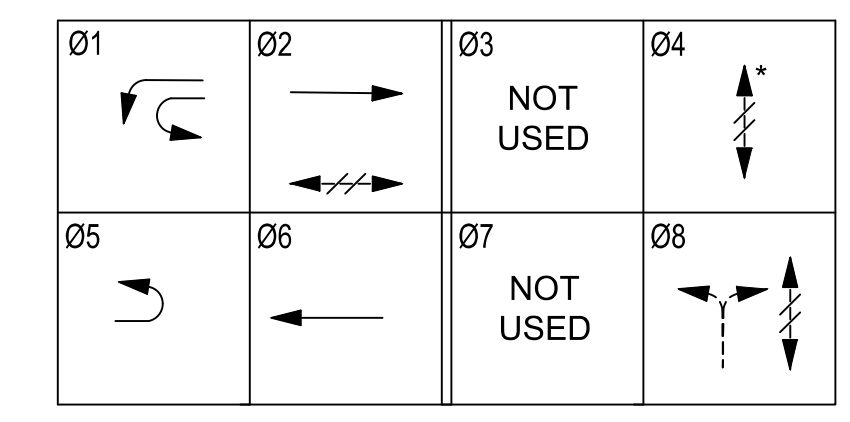
SIGNAL HEAD DISPLAY



NOTE:

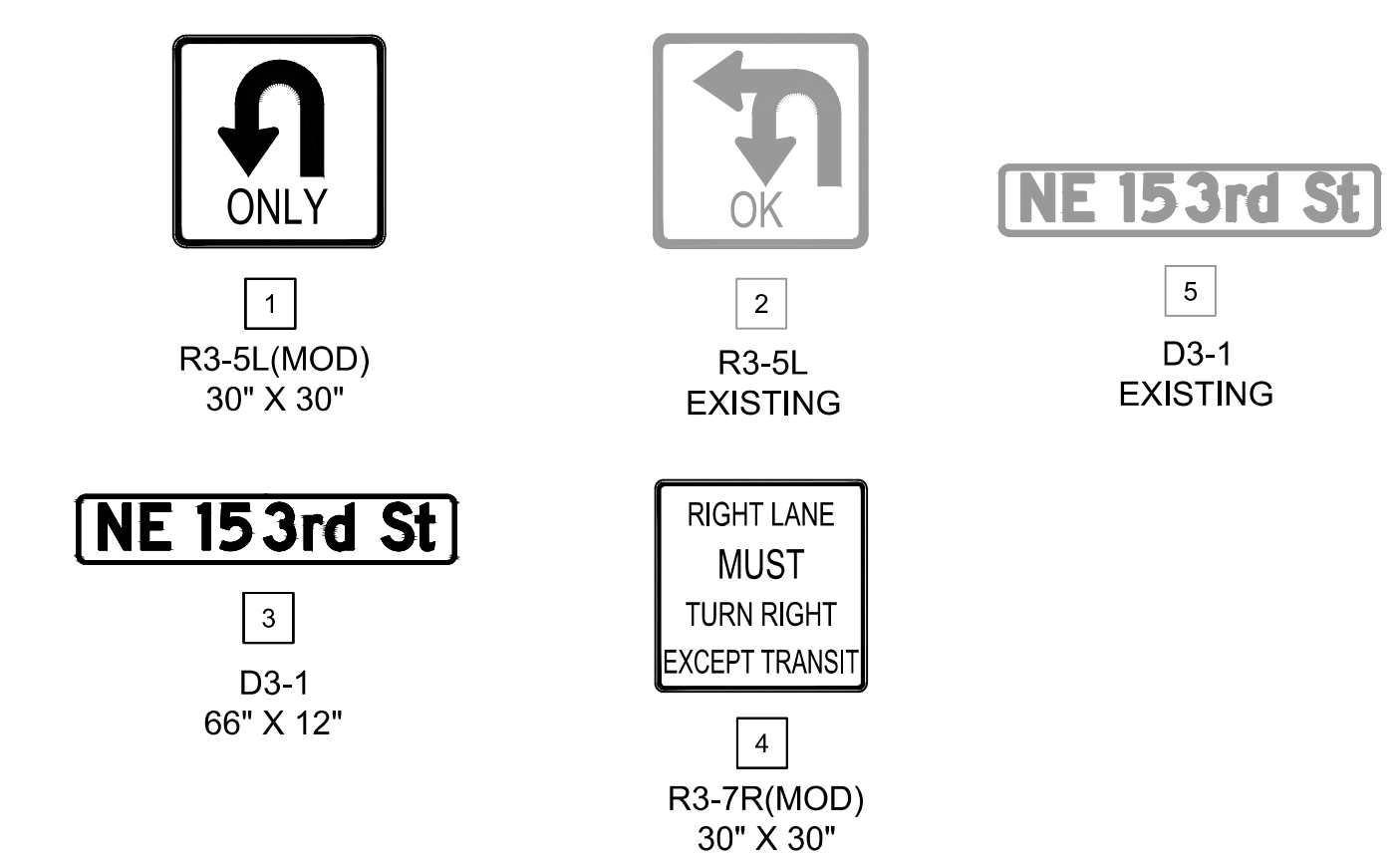
1. ALL NEW VEHICLE AND PEDESTRIAN SIGNAL HEADS SHALL BE LED TYPE. ALL NEW VEHICLE SIGNAL HEADS SHALL USE TUNNEL VISORS AND 5" BACKPLATES WITH 1" YELLOW RETROREFLECTIVE TAPE.
2. ALL NEW VEHICLE SIGNAL HEADS SHALL USE MOUNTING TYPE "M" ON TENONS PER WSDOT STD PLAN J-75.20-01, AND TYPE "D" PER WSDOT STD PLAN J-75.20-02. ALL NEW PEDESTRIAN SIGNAL HEADS SHALL USE TYPE "C" OR "D" PER WSDOT STD PLAN J-20.16-02, AND TYPE "E" PER WSDOT STD PLAN J-75.20-02.

PHASE SEQUENCE DIAGRAM



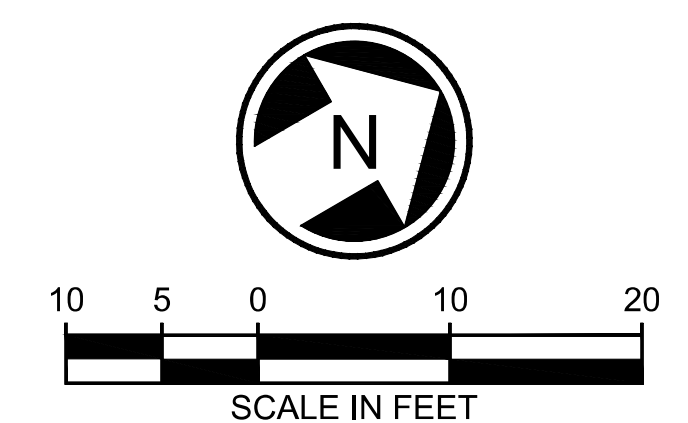
- PROTECTED VEHICLE MOVEMENT
- PERMISSIVE VEHICLE MOVEMENT
- PEDESTRIAN MOVEMENT
- * LEAD PEDESTRIAN INTERVAL (LPI) - AGENCY CONFIGURED

SIGN SCHEDULE



NOTES:

1. ALL SIGNS SHALL COMPLY WITH MUTCD 2009 STANDARDS AND WA STATE SUPPLEMENT.
2. ALL SIGN DIMENSIONS SHALL BE VERIFIED WITH THE ENGINEER AND MANUFACTURER PRIOR TO FABRICATION.
3. MAST ARM MOUNTED SIGNS SHALL HAVE TYPE IX SHEETING AND SHALL BE BRACED AND INSTALLED PER WSDOT STD. PLAN G-30.10-04.

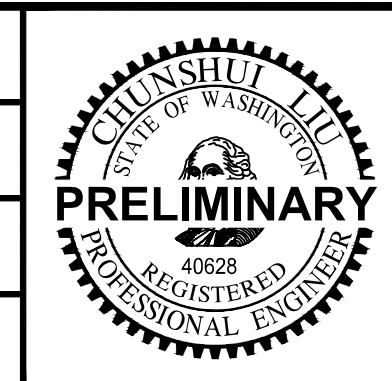


06/02/23 | 5:10 PM | REBECCA.SNELLINGS | C:\PWORKING\JACOBS_B\IN\EG_EX-SNELLIR\0403112\SS-BT320-TSD201-207.DWG

90% SUBMITTAL

NOT FOR CONSTRUCTION

DESIGNED BY:
C. TRAPP
 DRAWN BY:
R. SNELLINGS
 CHECKED BY:
B. NIEHOFF
 APPROVED BY:
J. LIU



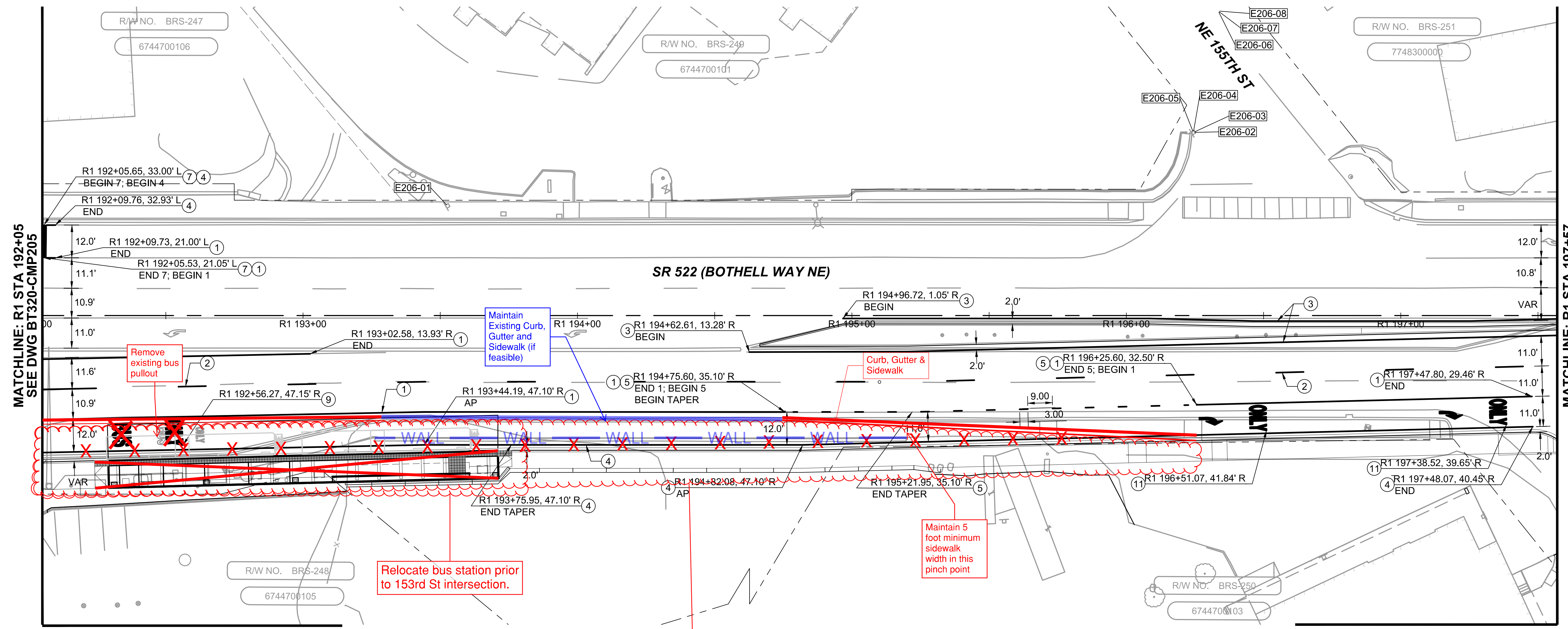
SUBMITTED BY: P. AVADUTHA | DATE: 06/09/2023 | REVIEWED BY: K. LEOTTA | DATE: 06/09/2023

SCALE: 1"=10'
 FILENAME: S3-BT320-TSD201-207
 CONTRACT No.: RTA/LR OA 0206-19
 DATE: 06/09/2023

SR 522 / NE 145TH BRT
SR 522 (BOTHELL WAY NE)
 BT306
 TRAFFIC SIGNAL DETAILS
 SR 522 (BOTHELL WAY NE)/NE 153RD ST
 LAKE FOREST PARK

DRAWING No.: **BT320-TSD201**
 FACILITY ID: BT320
 SHEET No.: 0869 | REV:

Xrefs:
 xS3-BT522-CRP301
 xS3-BT522-VRX101
 xBRT-CUD06-TB22x34
 xS3-BT522-VSF100
 xS3-BT522-GZK101
 GB-SEAL-RW044721
 xS3-BT522-CMP301
 xS3-BT522-CMN301
 xS3-BT522-CAP301
 xS3-BT523-CAP201
 xS3-BT523-CMP201
 xS3-BT523-CRP201
 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZN001



MATCHLINE: R1 STA 192+05
SEE DWG BT320-CMP205

MATCHLINE: R1 STA 197+57
SEE DWG BT322-CMP207

MATCHLINE: NE 153RD ST
SEE DWG BT320-CMP222

MATCHLINE: NE 155TH ST
SEE DWG BT320-CMP228

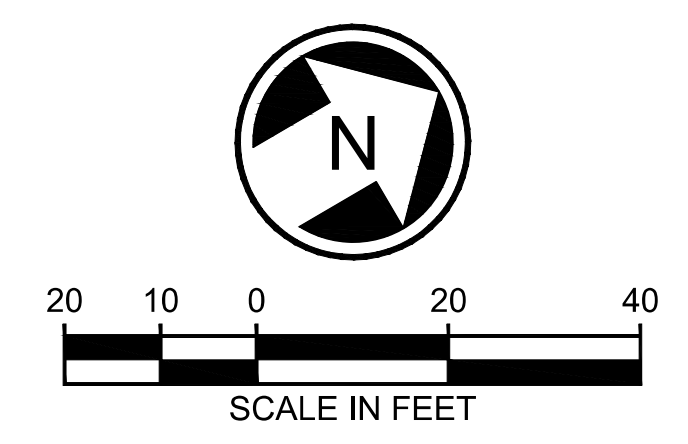
NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
 - ⑨ "BUS ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10
 - ⑪ TYPE 2SR (RIGHT) TRAFFIC ARROW AND "ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10




Shift sidewalk, curb, and edge of pavement improvements up to parallel proposed edge line. Maintain existing back of walk across creek with minor modifications only as necessary to complete pedestrian connections.

Suggested Modifications

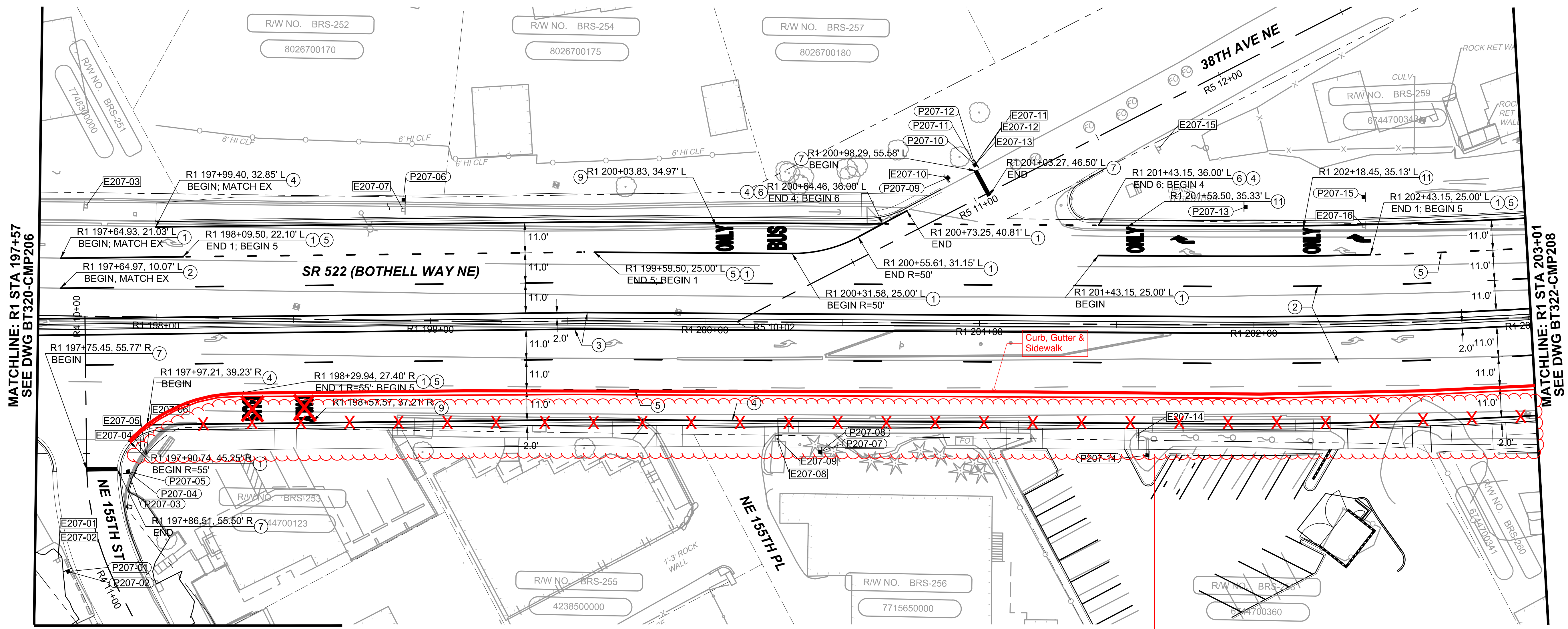
1. Relocate bus stop prior to intersection.
2. Revise roadway, curb, and sidewalk geometry to remove BAT lane while maintaining right turn lane to reduce ROW impacts.
3. Revise sidewalk width to match existing creek crossing structural wall at the back of walk.



05/25/23 | 4:09 PM | NGUYEN
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<h1 style="margin: 0;">90% SUBMITTAL</h1> <h2 style="margin: 0;">NOT FOR CONSTRUCTION</h2>	DESIGNED BY: D. TORREY DRAWN BY: O. NGUYEN CHECKED BY: M. KHURSHIED APPROVED BY: L. OLSEN		Jacobs		LINE IS 1" AT FULL SCALE		SCALE: 1"=20' FILENAME: S3-BT320-CMP201-210 CONTRACT No.: RTA/LR OA 0206-19 DATE: 06/09/2023	SR 522 / NE 145TH BRT SR 522 (BOTHELL WAY NE) BT306 CIVIL PAVEMENT MARKING AND SIGNAGE PLAN R1 STA 192+05 TO R1 STA 197+57	DRAWING No.: BT320-CMP206 FACILITY ID: BT320 SHEET No.: 0734 REV:
	SUBMITTED BY: P. AVADUTHA DATE: 06/09/2023 REVIEWED BY: K. LEOTTA DATE: 06/09/2023								

Xrefs:
 xS3-BT522-CRP301
 xS3-BT522-VRX101
 xBRT-CUD06-TB22x34
 xS3-BT522-VSF100
 xS3-BT522-GZK101
 xS3-BT522-GZK101
 xS3-BT522-CAP301
 xS3-BT522-CMP301
 xS3-BT522-CMN301
 xS3-BT522-CAP301
 xS3-BT523-CAP201
 xS3-BT523-CRP201
 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZM001



MATCHLINE: R1 STA 197+57
SEE DWG BT320-CMP206

MATCHLINE: R1 STA 203+01
SEE DWG BT322-CMP208

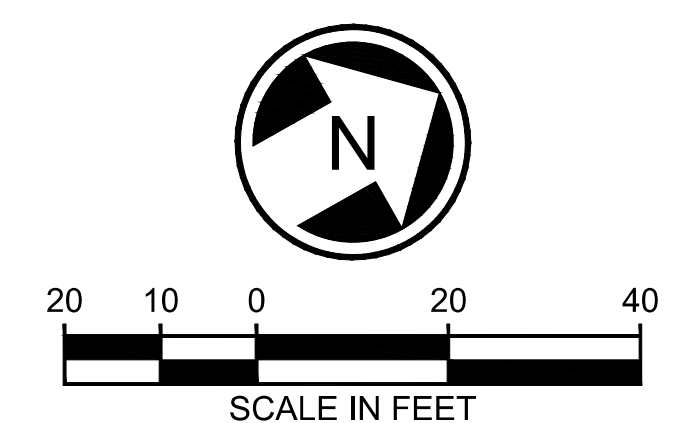
MATCHLINE: R4 STA 11+16
SEE DWG BT322-CMP228

Shift sidewalk, curb, and edge of pavement improvements to align with proposed edge of thru lane.

Suggested Modifications
 1. Revise roadway, curb, and sidewalk geometry to remove BAT lane to reduce ROW impacts and preserve mature vegetation as much as feasible.

NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
 - ⑥ WHITE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
 - ⑦ STOP LINE PER WSDOT STD. PLAN M-24.60
 - ⑧ "BUS ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10
 - ⑨ TYPE 2SR (RIGHT) TRAFFIC ARROW AND "ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10



05/25/23 | 4:45 PM | NGUYEN
 C:\P\WORKING\JACOBS_BRI\EG_NGUYEN\040312\SS-BT320-CMP201-210.DWG

90% SUBMITTAL
 NOT FOR CONSTRUCTION

DESIGNED BY:
D. TORREY
 DRAWN BY:
O. NGUYEN
 CHECKED BY:
M. KHURSHIED
 APPROVED BY:
L. OLSEN



Jacobs

STRIDE
 BRT General Engineering Consultant

LINE IS 1" AT FULL SCALE



SCALE:
1"=20'
 FILENAME:
S3-BT320-CMP201-210
 CONTRACT No.:
RTA/LR OA 0206-19
 DATE:
06/09/2023

SUBMITTED BY:
P. AVADUTHA

DATE:
06/09/2023

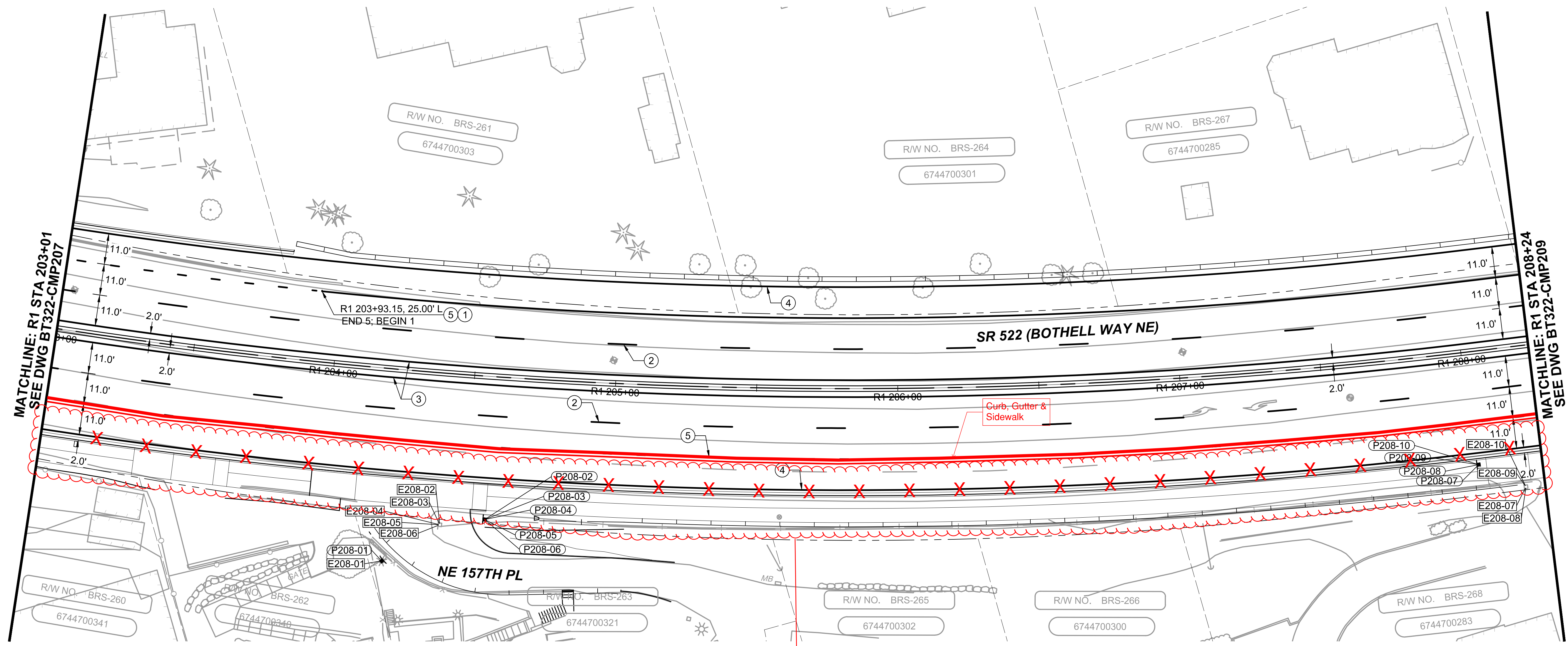
REVIEWED BY:
K. LEOTTA

DATE:
06/09/2023

SR 522 / NE 145TH BRT
SR 522 (BOTHELL WAY NE)
 BT306
 CIVIL
 PAVEMENT MARKING AND SIGNAGE PLAN
 R1 STA 197+57 TO R1 STA 203+01

DRAWING No.:
BT322-CMP207
 FACILITY ID:
BT322
 SHEET No.:
0735
 REV:

Xrefs:
 xS3-BT522-CRP301
 xS3-BT522-VRX101
 xBRT-CUD06-TB22x34
 xS3-BT522-VSF100
 xS3-BT522-GZK101
 GB-SEAL-RW044721
 xS3-BT522-CMP301
 xS3-BT522-CMN301
 xS3-BT522-CAP301
 xS3-BT523-CAP201
 xS3-BT523-CMP201
 xS3-BT523-CRP201
 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZN001

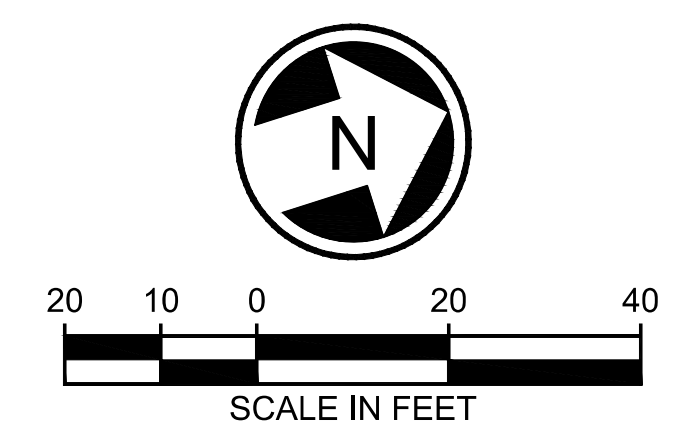


NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.



- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10

Shift sidewalk, curb, and edge of pavement improvements up to more closely align with existing roadway limits.

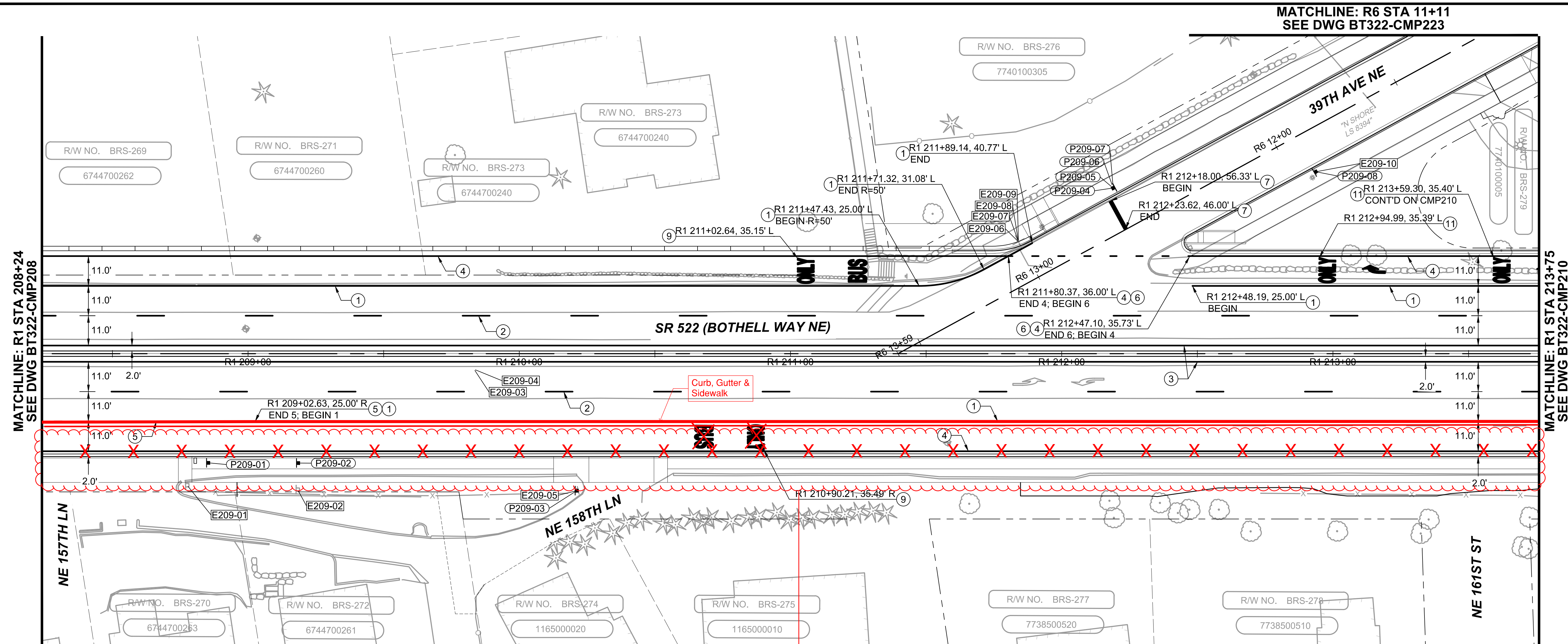
Suggested Modifications
 1. Revise roadway, curb, and sidewalk geometry to remove BAT lane to reduce ROW impacts and preserve mature vegetation as much as feasible.



05/25/23 | 4:29 PM | NGUYEN
 C:\P\WORKING\JACOBS_BRI\EG_NGUYEN\0403112\SS-BT320-CMP201-210.DWG

90% SUBMITTAL	DESIGNED BY: D. TORREY		Jacobs	STRIDE BRT General Engineering Consultant	LINE IS 1" AT FULL SCALE		SCALE: 1"=20'	SR 522 / NE 145TH BRT SR 522 (BOTHELL WAY NE) BT306 CIVIL PAVEMENT MARKING AND SIGNAGE PLAN R1 STA 203+01 TO R1 STA 208+24	DRAWING No.: BT322-CMP208
	DRAWN BY: O. NGUYEN						FILENAME: S3-BT320-CMP201-210		FACILITY ID: BT322
CHECKED BY: M. KHURSHIED	CONTRACT No.: RTA/LR OA 0206-19						SHEET No.: REV:		
APPROVED BY: L. OLSEN	DATE: 06/09/2023						DATE: 06/09/2023		0736
NOT FOR CONSTRUCTION	SUBMITTED BY: P. AVADUTHA								

Xrefs:
 xS3-BT522-CRP301
 xS3-BT522-VRX101
 xBRT-CUD06-TB22x34
 xS3-BT522-VSF100
 xS3-BT522-GZK101
 GB-SEAL-RW044721
 xS3-BT522-CMP301
 xS3-BT522-CMN301
 xS3-BT522-CAP301
 xS3-BT523-CAP201
 xS3-BT523-CMP201
 xS3-BT523-CRP201
 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZM001

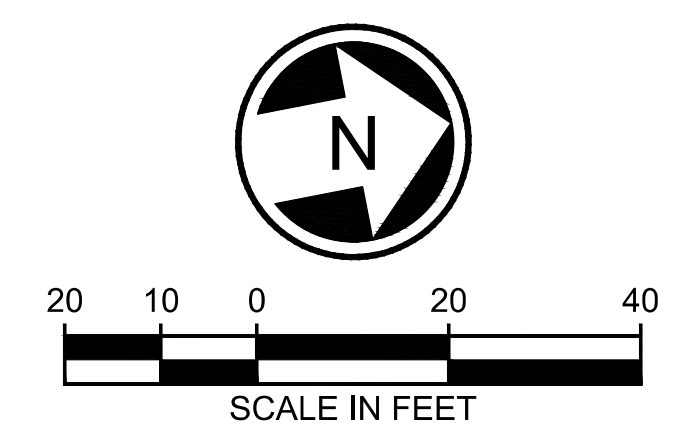


NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
 - ⑥ WHITE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
 - ⑦ STOP LINE PER WSDOT STD. PLAN M-24.60
 - ⑨ "BUS ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10
 - ⑪ TYPE 2SR (RIGHT) TRAFFIC ARROW AND "ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10

Shift sidewalk, curb, and edge of pavement improvements up to more closely align with existing roadway limits.

Suggested Modifications
 1. Revise roadway, curb, and sidewalk geometry to remove BAT lane to reduce ROW impacts and preserve mature vegetation as much as feasible.



05/25/23 | 4:30 PM | NGUYEN
 C:\P\WORKING\JACOBS_BRI\EG_NGUYEN\0403112\SS-BT320-CMP201-210.DWG

90% SUBMITTAL
 NOT FOR CONSTRUCTION

DESIGNED BY:
D. TORREY
 DRAWN BY:
O. NGUYEN
 CHECKED BY:
M. KHURSHEED
 APPROVED BY:
L. OLSEN



Jacobs

STRIDE
 BRT General Engineering Consultant

LINE IS 1" AT FULL SCALE



SCALE:
1"=20'
 FILENAME:
S3-BT320-CMP201-210
 CONTRACT No.:
RTA/LR OA 0206-19
 DATE:
06/09/2023

SUBMITTED BY:
P. AVADUTHA

DATE:
06/09/2023

REVIEWED BY:
K. LEOTTA

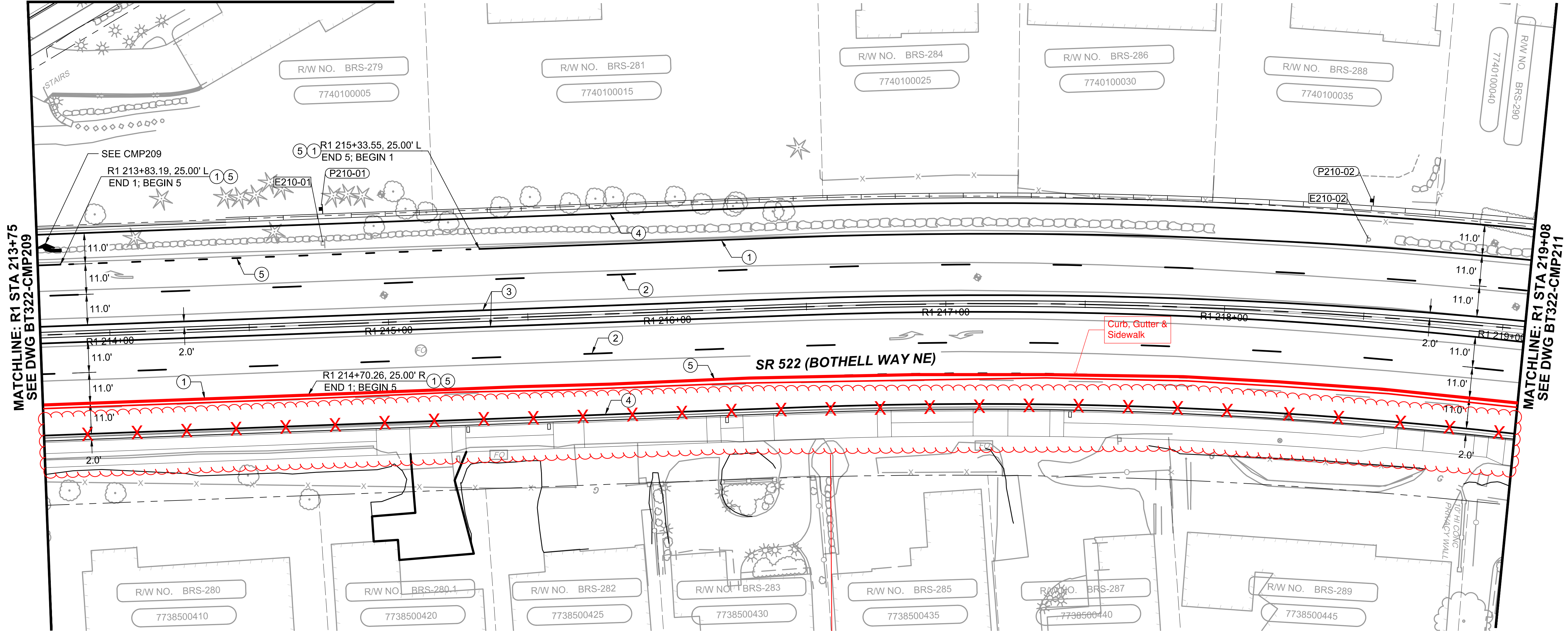
DATE:
06/09/2023

SR 522 / NE 145TH BRT
SR 522 (BOTHELL WAY NE)
 BT306
 CIVIL
 PAVEMENT MARKING AND SIGNAGE PLAN
 R1 STA 208+24 TO R1 STA 213+75

DRAWING No.:
BT322-CMP209
 FACILITY ID:
BT322
 SHEET No.:
0737
 REV:

Xrefs:
 xS3-BT522-CRP301
 xS3-BT522-VRX101
 xBRT-CUD06-TB2234
 xS3-BT522-VSF100
 xS3-BT522-GZK101
 GB-SEAL-RW044721
 xS3-BT522-CMP301
 xS3-BT522-CMN301
 xS3-BT522-CAP301
 xS3-BT523-CAP201
 xS3-BT523-CRP201
 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZN001

MATCHLINE: 39TH AVE NE
 SEE DWG BT322-CMP223

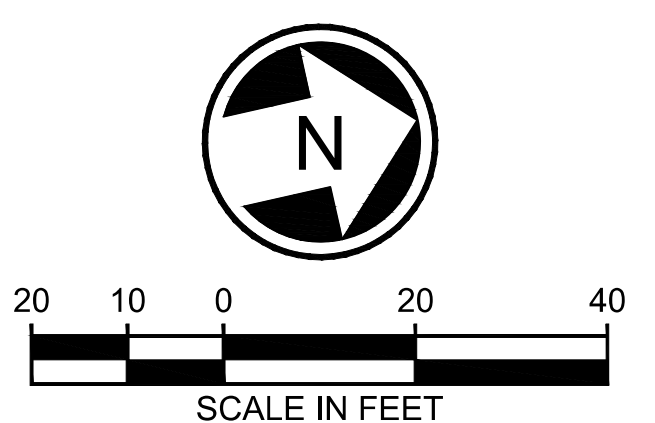


NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10

Shift sidewalk, curb, and edge of pavement improvements up to more closely align with existing roadway limits.

Suggested Modifications
 1. Revise roadway, curb, and sidewalk geometry to remove BAT lane to reduce ROW impacts and preserve mature vegetation as much as feasible.



05/30/23 | 12:15 PM | NGUYEN
 C:\P\WORKING\JACOBS_BR\JEG_NGUYEN\0403112\SS-BT320-CMP201-210.DWG

90% SUBMITTAL
 NOT FOR CONSTRUCTION

DESIGNED BY:
D. TORREY
 DRAWN BY:
O. NGUYEN
 CHECKED BY:
M. KHURSHEED
 APPROVED BY:
L. OLSEN



Jacobs

STRIDE
 BRT General Engineering Consultant

LINE IS 1" AT FULL SCALE



SCALE:
1"=20'
 FILENAME:
S3-BT320-CMP201-210
 CONTRACT No.:
RTA/LR OA 0206-19
 DATE:
06/09/2023

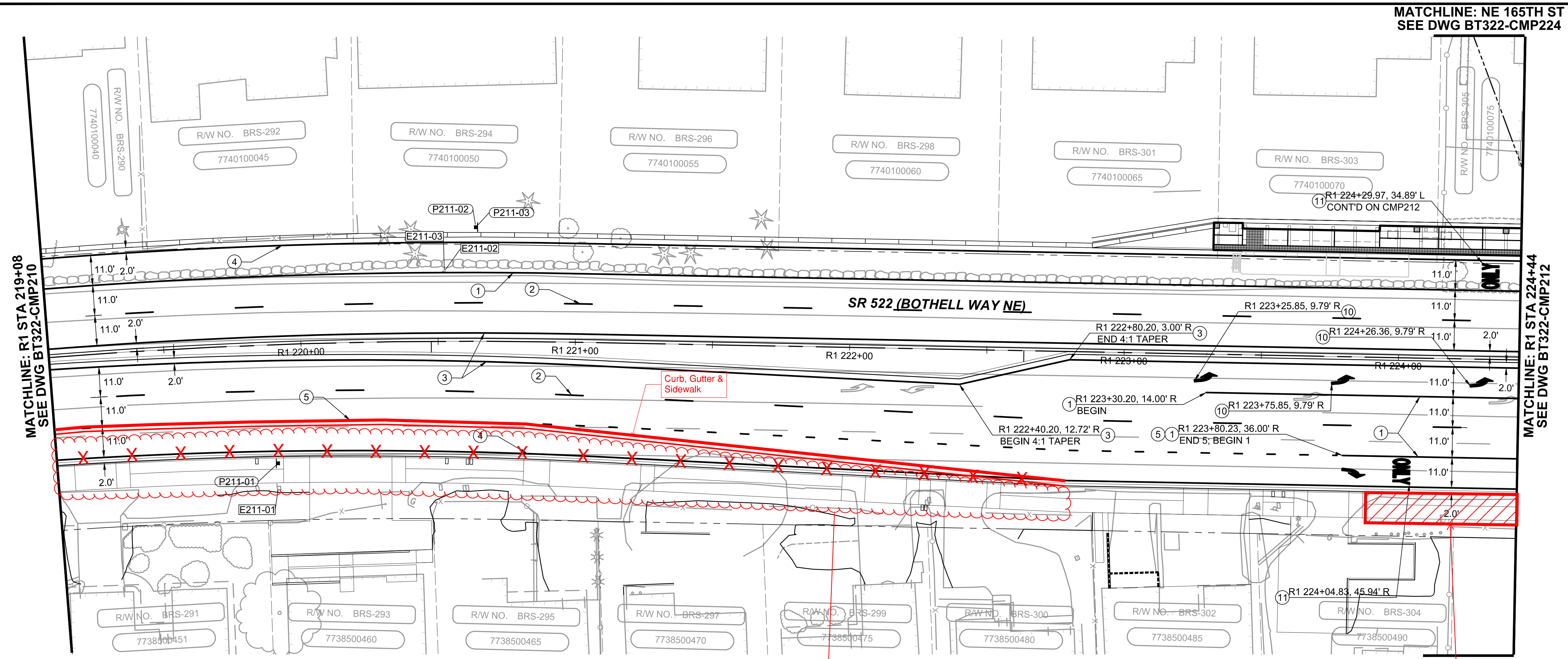
SUBMITTED BY:
P. AVADUTHA
 DATE:
06/09/2023

REVIEWED BY:
K. LEOTTA

SR 522 / NE 145TH BRT
SR 522 (BOTHELL WAY NE)
 BT306
 CIVIL
 PAVEMENT MARKING AND SIGNAGE PLAN
 R1 STA 213+75 TO R1 STA 219+08

DRAWING No.:
BT322-CMP210
 FACILITY ID:
BT322
 SHEET No.:
0738
 REV:

Xrefs:
 xS3-BT522-CAP301
 xS3-BT522-CRP301
 xS3-BT522-VSF100
 xS3-BT522-VRX101
 xBRT-CUD06-TB22x34
 xS3-BT522-GZK101
 GB-35AL-RWD4721
 xS3-BT522-CMP301
 xS3-BT522-CMN301
 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZN001



MATCHLINE: NE 165TH ST
 SEE DWG BT322-CMP224

MATCHLINE: R1 STA 219+08
 SEE DWG BT322-CMP210

MATCHLINE: R1 STA 224+44
 SEE DWG BT322-CMP212

MATCHLINE: NE 165TH ST
 SEE DWG BT322-CMP225

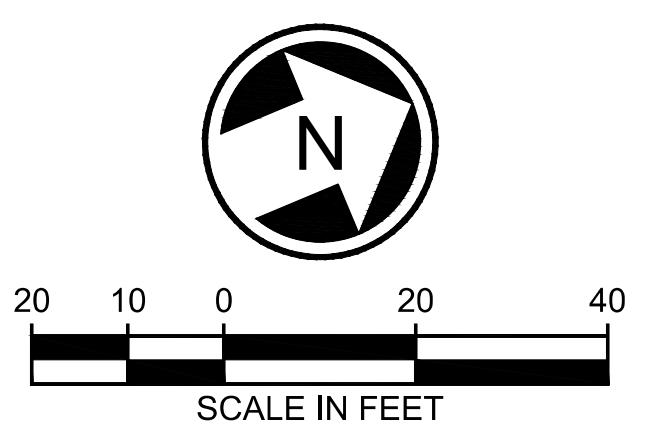
NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
 - ⑩ TYPE 2SL (LEFT) TRAFFIC ARROW PER WSDOT STD. PLAN M-24.40
 - ⑪ TYPE 2SR (RIGHT) TRAFFIC ARROW AND "ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10

Shift sidewalk, curb, and edge of pavement improvements up to more closely align with existing roadway limits.

Relocate proposed bus station from opposite side of intersection.

Suggested Modifications
 1. Revise roadway, curb, and sidewalk geometry to remove BAT lane while maintaining right turn lane to reduce ROW impacts and preserve mature vegetation as much as feasible.

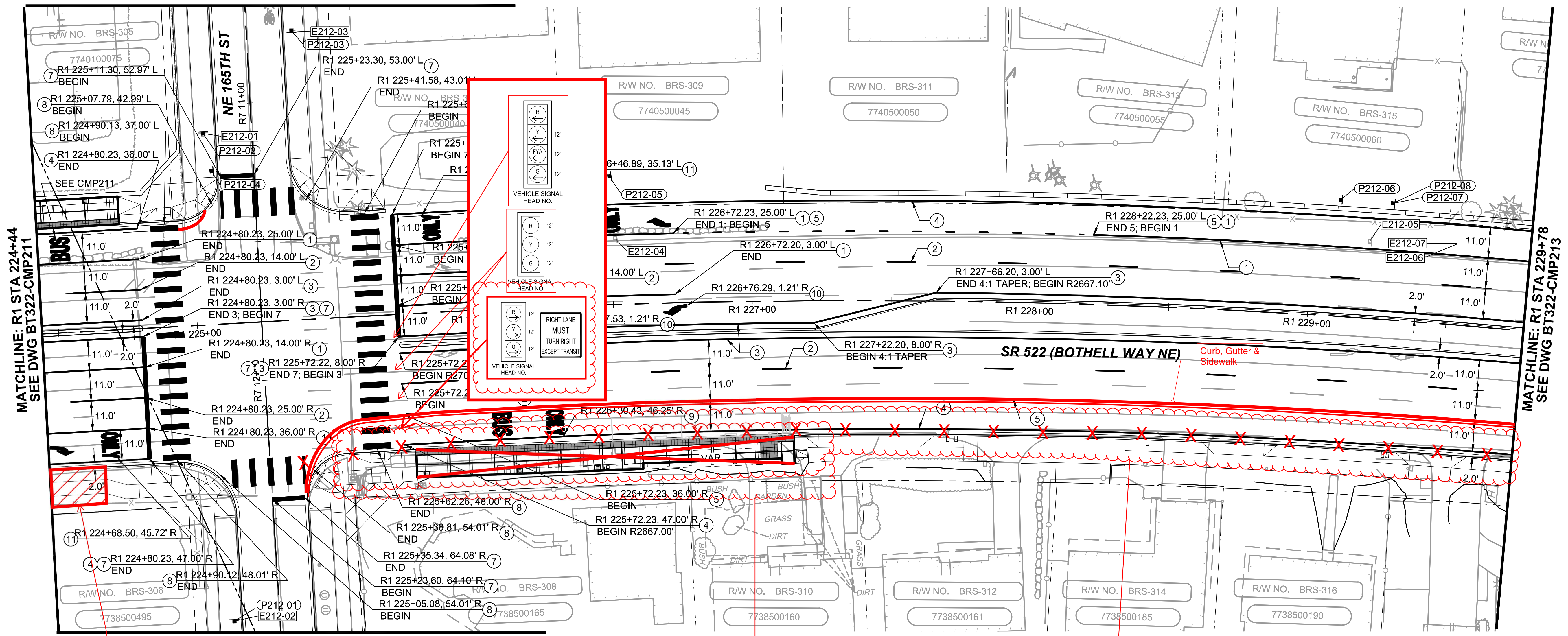


05/25/23 | 3:03 PM | NGUYEN
 C:\PWORKING\JACOBS_BRI\EG_NGUYEN\0403112\SS-BT322-CMP211-220.DWG

90% SUBMITTAL	DESIGNED BY: D. TORREY		Jacobs	STRIDE BRT General Engineering Consultant	LINE IS 1" AT FULL SCALE		SCALE: 1"=20'	SR 522 / NE 145TH BRT SR 522 (BOTHELL WAY NE) BT306	DRAWING No.: BT322-CMP211
	NOT FOR CONSTRUCTION						DRAWN BY: O. NGUYEN		FACILITY ID: BT322
	CHECKED BY: M. KHURSHEED						SHEET No.: 0739		
	APPROVED BY: L. OLSEN						REV:		
	SUBMITTED BY: P. AVADUTHA	DATE: 06/09/2023	REVIEWED BY: K. LEOTTA	DATE: 06/09/2023		DATE: 06/09/2023			

MATCHLINE: R7 STA 10+65
SEE DWG BT322-CMP224

Xrefs:
xS3-BT522-CAP301
xS3-BT522-CRP301
xS3-BT522-VSF100
xS3-BT522-VRX101
xBR1-CUD06-TB22-34
xS3-BT522-GZK101
GB-SEAL-RWD4721
xS3-BT522-CMP301
xS3-BT522-CMN301
xS3-BT522-CMP302
xS3-BT522-ASP301
xS3-BT522-CZN001



Relocate proposed bus station from opposite side of intersection.

MATCHLINE: R7 STA 12+91
SEE DWG BT322-CMP225

Relocate bus station prior to NE 165th St intersection.

Shift sidewalk, curb, and edge of pavement improvements up to more closely align with existing roadway limits.

Suggested Modifications
 1. Relocate bus stop prior to intersection to improve traffic flow with removed BAT lane.
 2. Revise right turn lane signal to right arrows to allow for transit queue jump.
 3. Revise roadway, curb, and sidewalk geometry to remove BAT lane north of NE 165th St intersection

NOTES:

1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

CONSTRUCTION NOTES:

- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
- ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
- ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
- ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
- ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
- ⑦ STOP LINE PER WSDOT STD. PLAN M-24.60
- ⑧ CROSSWALK LINE PER WSDOT STD. PLAN M-15.10
- ⑨ "BUS ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10
- ⑩ TYPE 2SL (LEFT) TRAFFIC ARROW PER WSDOT STD. PLAN M-24.40

CONSTRUCTION NOTES CONT'D:

- ⑪ TYPE 2SR (RIGHT) TRAFFIC ARROW AND "ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10

90% SUBMITTAL

NOT FOR CONSTRUCTION

DESIGNED BY:
D. TORREY
DRAWN BY:
O. NGUYEN
CHECKED BY:
M. KHURSHIED
APPROVED BY:
L. OLSEN



Jacobs

STRIDE
BRT General Engineering Consultant

LINE IS 1" AT FULL SCALE



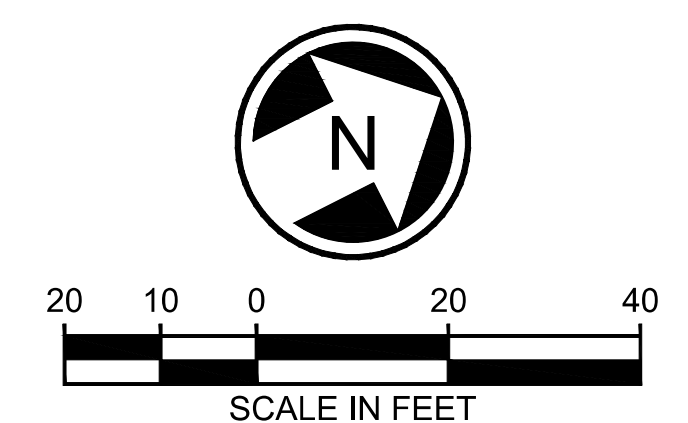
SCALE:
1"=20'
FILENAME:
S3-BT322-CMP211-220
CONTRACT No.:
RTA/LR OA 0206-19
DATE:
06/09/2023

SUBMITTED BY:
P. AVADUTHA
DATE:
06/09/2023

REVIEWED BY:
K. LEOTTA
DATE:
06/09/2023

SR 522 / NE 145TH BRT
SR 522 (BOTHELL WAY NE)
BT306
CIVIL
PAVEMENT MARKING AND SIGNAGE PLAN
R1 STA 224+44 TO R1 STA 229+78

DRAWING No.:
BT322-CMP212
FACILITY ID:
BT322
SHEET No.:
0740
REV:

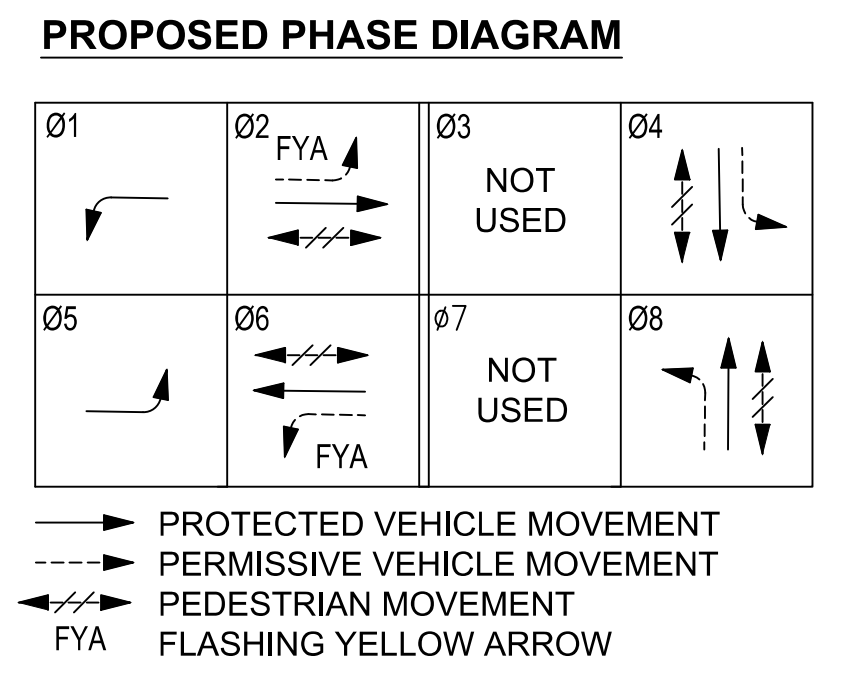
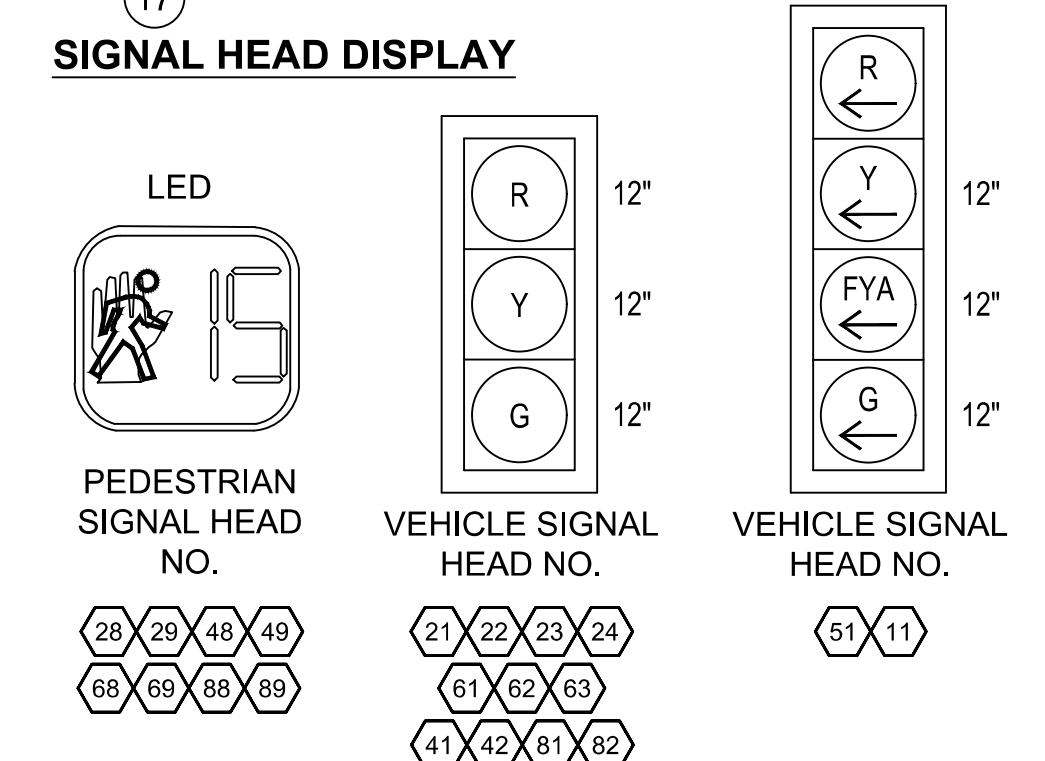
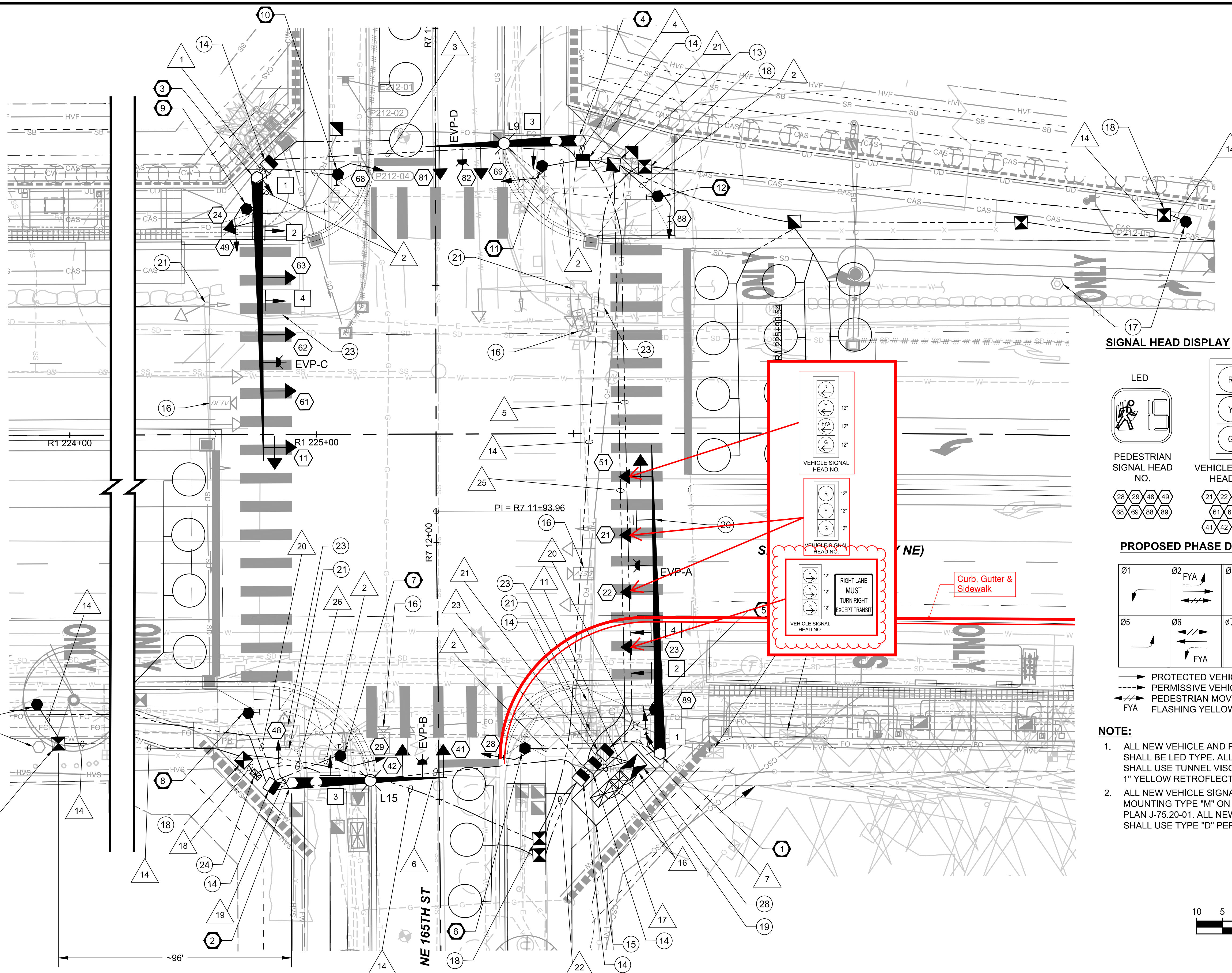


05/30/23 | 1:06 PM | NGUYEN O
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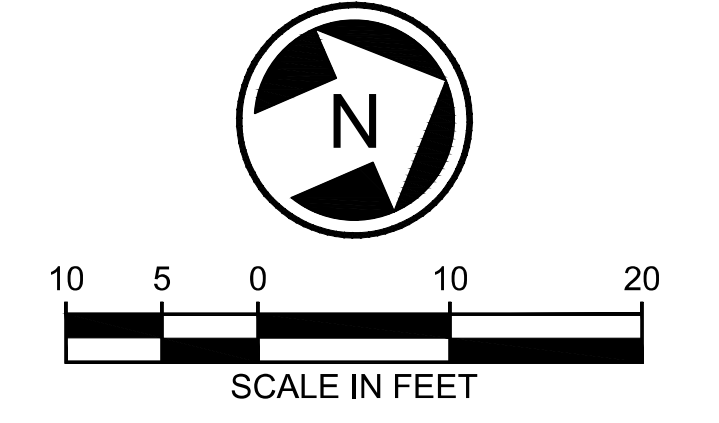
Xrefs:
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 xS3-BT522-VSF100
 xS3-BT522-VRX101
 xS3-BT522-LCP301
 xS3-BT522-TSP301
 xS3-BT522-RPP301
 xS3-BT522-CRP301
 xS3-BT522-CAP301
 xS3-BT522-CLP301
 xS3-BT522-CMP301
 xS3-BT522-CIP301
 xS3-BT522-APP301
 xS3-BT522-EPP301
 xS3-BT522-JIP301
 xS3-BT522-LPP301
 xS3-BT522-SWP301
 xS3-BT522-TIP301
 LFP Quantity Break-up

GENERAL NOTES:

- EXISTING VIDEO DETECTION CAMERAS TO BE REMOVED AND SALVAGED. ALL CAMERAS SHALL RETURN TO WSDOT.
- SEE SHEET BT320-TSD201 FOR LEGEND.
- SEE SHEET BT320-TSD204 FOR CONSTRUCTION NOTES
- SEE SHEET BT322-CLP208 FOR PROPOSED ILLUMINATION EQUIPMENT AND POWER SERVICE.
- SEE SHEET BT320-TSS206 FOR PROPOSED WIRING SCHEDULE.
- SEE SHEET BT320-TIP212 FOR PROPOSED COMMUNICATION EQUIPMENT.
- SEE SHEET BT320-TSD205 FOR ASSOCIATED DETECTION PLAN.
- SEE SHEET BT320-TSS209 & SEE SHEET BT320-TSS210 FOR TERMINATION SHEETS.



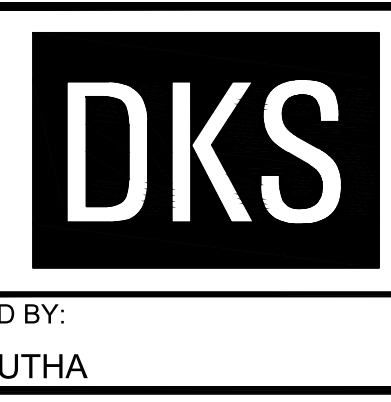
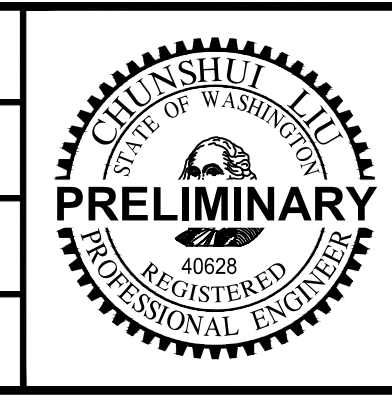
- NOTE:**
- ALL NEW VEHICLE AND PEDESTRIAN SIGNAL HEADS SHALL BE LED TYPE. ALL NEW VEHICLE SIGNAL HEADS SHALL USE TUNNEL VISORS AND 5" BACKPLATES WITH 1" YELLOW RETROFLECTIVE TAPE.
 - ALL NEW VEHICLE SIGNAL HEADS SHALL USE MOUNTING TYPE "M" ON TENONS PER WSDOT STD PLAN J-75.20-01. ALL NEW PEDESTRIAN SIGNAL HEADS SHALL USE TYPE "D" PER WSDOT STD PLAN J-20.16-02.



06/02/23 | 5:15 PM | REBECCA.SNELLINGS
 C:\PWORKING\JACOBS_B\IN\EG_EX-SNELLIR\0403112\33-BT320-TSD201-207.DWG

90% SUBMITTAL
 NOT FOR CONSTRUCTION

DESIGNED BY:
C. TRAPP
 DRAWN BY:
R. SNELLINGS
 CHECKED BY:
B. NIEHOFF
 APPROVED BY:
J. LIU



STRIDE
 BRT General Engineering Consultant

LINE IS 1" AT FULL SCALE



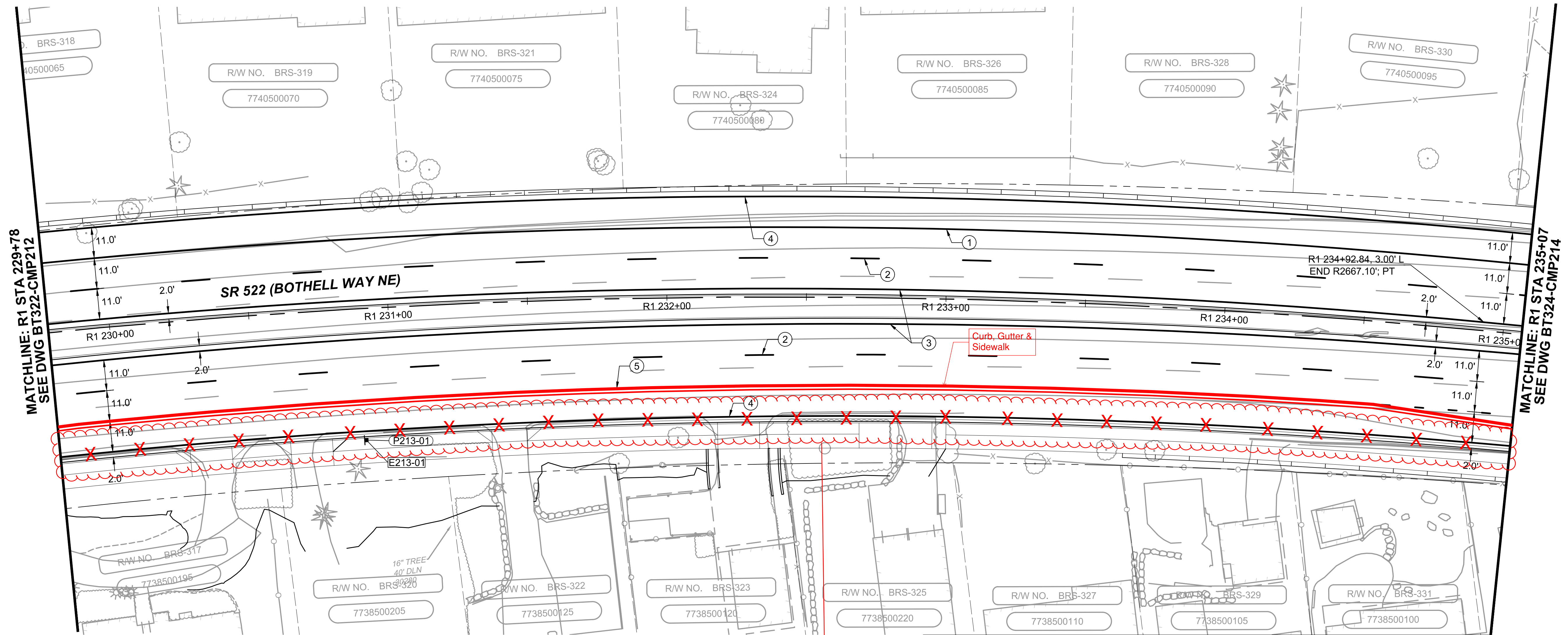
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S3-BT320-TSD201-207
 CONTRACT No.:
RTA/LR OA 0206-19
 DATE:
06/09/2023

SR 522 / NE 145TH BRT
SR 522 (BOTHELL WAY NE)
 BT306
 TRAFFIC SIGNAL DETAILS
 SR 522 (BOTHELL WAY NE)/NE 165TH ST
 LAKE FOREST PARK

DRAWING No.:
BT322-TSD203
 FACILITY ID:
BT322
 SHEET No.:
0876
 REV:

SUBMITTED BY: P. AVADUTHA
 DATE: 06/09/2023
 REVIEWED BY: K. LEOTTA
 DATE: 06/09/2023

Xrefs:
 xS3-BT522-CAP301
 xS3-BT522-CRP301
 xS3-BT522-VSF100
 xS3-BT522-VRX101
 xBRT-CUD06-TB22x34
 xS3-BT522-GZK101
 GB-SEAL-RWD4721
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 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZN001

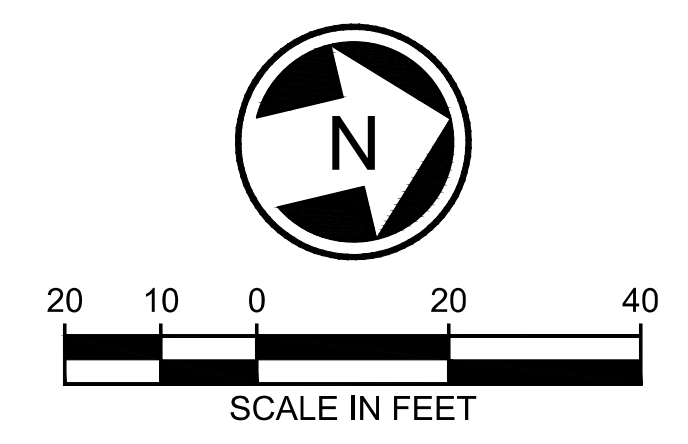


NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10

Shift sidewalk, curb, and edge of pavement improvements up to more closely align with existing roadway limits.

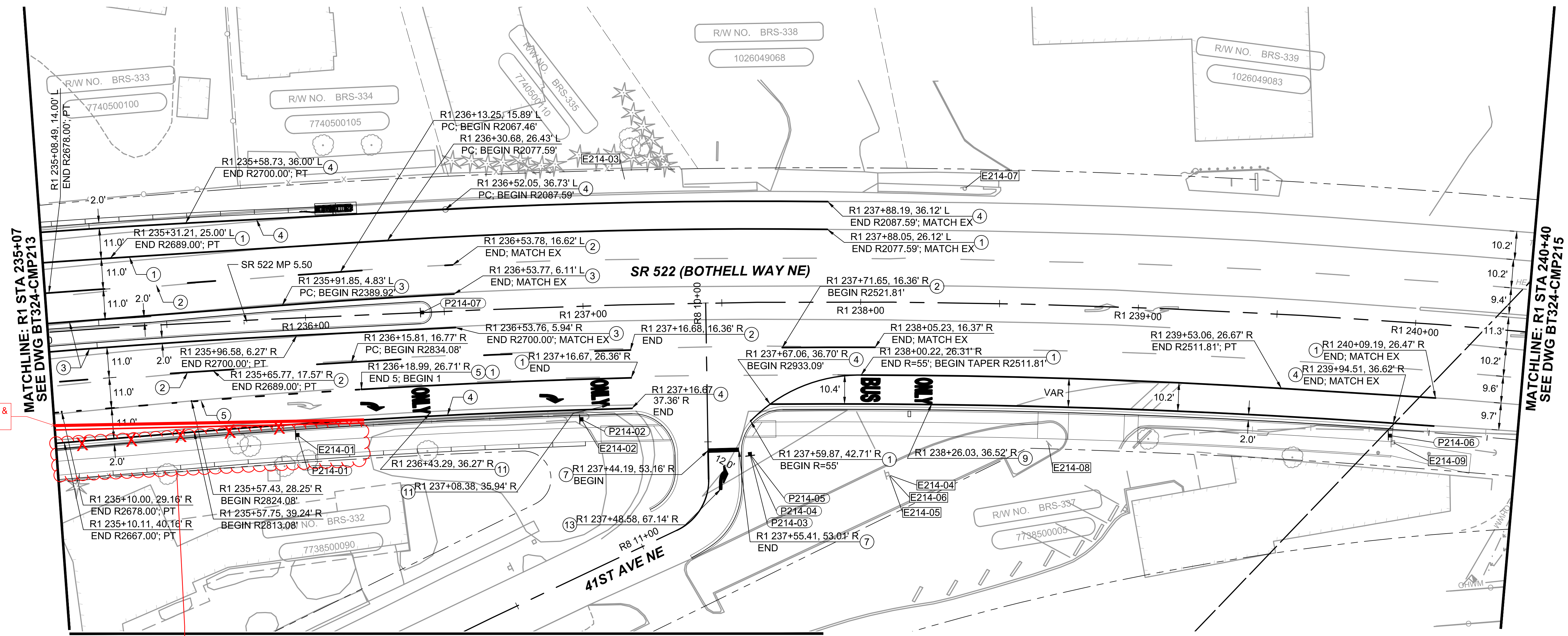
Suggested Modifications
 1. Revise roadway, curb, and sidewalk geometry to remove BAT lane while maintaining right turn lane to reduce ROW impacts and preserve mature vegetation as much as feasible.



05/25/23 | 3:07 PM | NGUYEN
 C:\PWORKING\JACOBS_B&JEG_NGUYEN\0403112\SS-BT322-CMP211-220.DWG

90% SUBMITTAL	DESIGNED BY: D. TORREY		Jacobs	STRIDE BRT General Engineering Consultant	LINE IS 1" AT FULL SCALE		SCALE: 1"=20'	SR 522 / NE 145TH BRT SR 522 (BOTHELL WAY NE) BT306	DRAWING No.: BT322-CMP213
	DRAWN BY: O. NGUYEN						FILENAME: S3-BT322-CMP211-220		FACILITY ID: BT322
NOT FOR CONSTRUCTION	CHECKED BY: M. KHURSHEED	REGISTERED PROFESSIONAL ENGINEER	SUBMITTED BY: P. AVADUTHA	DATE: 06/09/2023	REVIEWED BY: K. LEOTTA	DATE: 06/09/2023	CONTRACT No.: RTA/LR OA 0206-19	CIVIL	SHEET No.: 0741
	APPROVED BY: L. OLSEN						DATE: 06/09/2023	PAVEMENT MARKING AND SIGNAGE PLAN R1 STA 229+78 TO R1 STA 235+07	REV: 0741

Xrefs:
 xS3-BT522-CAP301
 xS3-BT522-CRP301
 xS3-BT522-VSF100
 xS3-BT522-VRX101
 xBRT-CUD06-TB22-34
 xS3-BT522-GZK101
 GB-SEAL-RWD4721
 xS3-BT522-CMP301
 xS3-BT522-CMN301
 xS3-BT522-CMP302
 xS3-BT522-ASP301
 xS3-BT522-CZN001



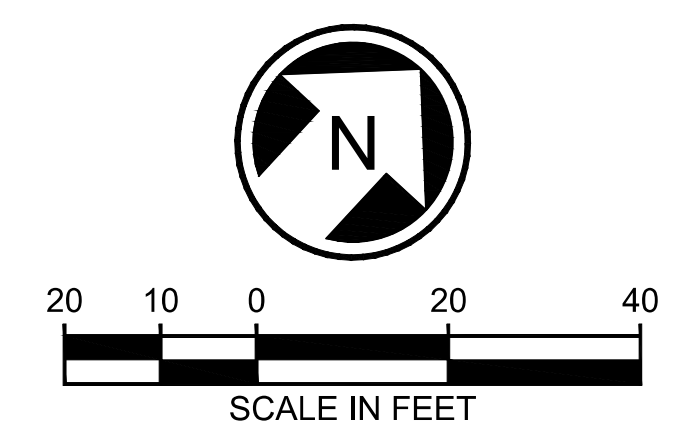
Shift sidewalk, curb, and edge of pavement improvements up to more closely align with existing roadway limits.

NOTES:
 1 FOR FULL LIST OF CONSTRUCTION NOTES AND GENERAL NOTES, SEE DRAWING BT320-CMN201 AND BT320-CMN202.

- CONSTRUCTION NOTES:**
- ① WHITE WIDE SOLID LANE LINE PER WSDOT STD. PLAN M-20.10
 - ② WHITE LANE LINE PER WSDOT STD. PLAN M-20.10
 - ③ YELLOW EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ④ WHITE EDGE LINE PER WSDOT STD. PLAN M-20.10
 - ⑤ WHITE WIDE DOTTED LANE LINE PER WSDOT STD. PLAN M-20.10
 - ⑦ STOP LINE PER WSDOT STD. PLAN M-24.60
 - ⑨ "BUS ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10
 - ⑩ TYPE 2SL (LEFT) TRAFFIC ARROW PER WSDOT STD. PLAN M-24.40
 - ⑪ TYPE 2SR (RIGHT) TRAFFIC ARROW AND "ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10

⑬ TYPE 2SR (RIGHT) TRAFFIC ARROW AND "ONLY" LOW SPEED TRAFFIC LETTERS PER WSDOT STD. PLAN M-80.10

Suggested Modifications
 1. Revise roadway, curb, and sidewalk geometry to remove BAT lane, up to 41st Ave NE while maintaining right turn lane to reduce ROW impacts and preserve mature vegetation as much as feasible.



05/25/23 | 5:00 PM | NGUYEN
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90% SUBMITTAL
 NOT FOR CONSTRUCTION

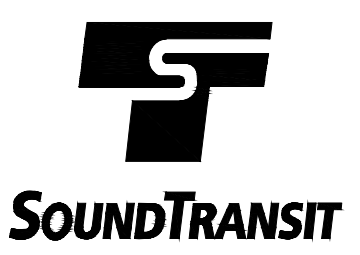
DESIGNED BY:
D. TORREY
 DRAWN BY:
O. NGUYEN
 CHECKED BY:
M. KHURSHIED
 APPROVED BY:
L. OLSEN



Jacobs

STRIDE
 BRT General Engineering Consultant

LINE IS 1" AT FULL SCALE



SCALE:
1"=20'
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S3-BT322-CMP211-220
 CONTRACT No.:
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 DATE:
06/09/2023

SUBMITTED BY:
P. AVADUTHA
 DATE:
06/09/2023

REVIEWED BY:
K. LEOTTA

SR 522 / NE 145TH BRT
SR 522 (BOTHELL WAY NE)
 BT306
 CIVIL
 PAVEMENT MARKING AND SIGNAGE PLAN
 R1 STA 235+07 TO R1 STA 240+40

DRAWING No.:
BT324-CMP214
 FACILITY ID:
BT324
 SHEET No.:
0742
 REV: